



Charles D. Baker, Governor
Karyn E. Polito, Lieutenant Governor
Stephanie Pollack, Secretary & CEO
Jonathan L. Gulliver, Acting Highway Administrator



March 11, 2021

605356-113892

ADDENDUM NO. 2

To Prospective Bidders and Others on:

WILLIAMSTOWN
Federal Aid Project No. NHP(BR-ON)-003S(205)X
Bridge Replacement and Related Work Br. No. W-37-015 (NEXT F Beams)
Route 2 (Main Street) over the Green River
(Re-Advertised Project)

BIDS TO BE OPENED AND READ: TUESDAY, MARCH 16, 2021 at 2:00 P.M.

Transmitting revisions to the Contract Documents as follows:

RESPONSES TO BIDDER'S QUESTIONS: Three pages.

DOCUMENT A00801: Revised Page 45.

PLANS: Revised sheets 26 and 45 of 58 (Bridge Title Sheet has been signed).

Take note of the above, substitute the revised page and sheets for the originals, and acknowledge Addendum No.2 in your Expedite Proposal file before submitting your bid.

Very truly yours,

Eric M. Cardone
Digitally signed
by Eric M.
Cardone
Date: 2021.03.11
10:36:30 -05'00'

Eric M. Cardone, P.E.
Construction Contracts Engineer

MB

c: Harry Adolphe, Project Manager

Ten Park Plaza, Suite 4160, Boston, MA 02116
Tel: 857-368-4636, TTY: 857-368-0655
www.mass.gov/massdot

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(605356-113892)

Responses to Bidder's Questions

Addendum No. 2, March 11, 2021

J.H. Maxymillian, Inc., e-mail dated March 8, 2021

- Question 1) The Special Provision for Item 101.01 Clearing and Grubbing states the estimated clearing and grubbing upset area is 0.4 acre. Is this area of clearing we should carry in our bid? If additional clearing is required for the work, how will the contractor be compensated for additional area?
- Response 1) Item 101.01 is a lump sum item. The upset area is 0.4 acre so the actual area may be less, but it should not be more than 0.4 acre.
- Question 2) The special provisions for item 127.1 State "Any temporary earth support, earth excavation, and gravel borrow for backfilling required to perform the demolition of these items shall be considered incidental to the project." This work overlaps with the temporary earth support, excavation and backfill that is required to install the new bridge. We are concerned this statement may be misunderstood and the contractor may not be paid for the temporary earth support, excavation and backfill required to for the construction of the new bridge items. Could you please provide additional clarification on this statement?
- Response 2) See revised Special Provisions, page A00801-45.
- Question 3) All the work related to the water lines, valves, hydrant extensions, etc. must done in accordance with the Williamstown Department of Public Works Standards. Will Sprinkler Fitters be required for the installation or testing of any of this water work?
- Response 3) Sprinkler fitters are not required.
- Question 4) Could you please provide some additional clarification on the work required in order to meet Access Restraints #5 & #6?
- Response 4) Access Restraint #5 is for the underground conduit installation. This for the underground electric conduit bank and manhole on the north side of the bridge (betterment work). Access Restraint #6 is for utility pole and overhead wire relocation to the permanent location.

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- Question 5) Drawing 45 of 58 (20 of 29 in the Bridge Plans) provide a cofferdam and sandbag design data table that provides various elevations for the cofferdam and sandbags. Note #2 under 'Hydraulic Notes', on the same plan, provides minimum elevations for the cofferdam. The minimum elevations under note #2 are lower than the elevations provided in the table. Also the table does not seem to provide locations that these elevation would relate to. Could you please provide some additional information/clarification on these elevations, specifically in the table? There seems to be some missing information.
- Response 5) See revised Plan, Sheet 45 of 58. The letters in the column headers, [A] through [F], correspond to the letters in the "Anticipated Cofferdam Detail" above the table.
- Question 6) Drawing 45 of 58, Note #3 under 'Hydraulic Notes' calls for the cofferdam to be removed with a vibratory hammer. The stage one cofferdam remains in place while we work on stage 2. This means that the stage 1 cofferdam is in place when the stage one beams and new center pier are in place. This will make removing the stage one cofferdam very difficult on the east side of the center pier and almost impossible on the west side of the center pier. The special provisions for the cofferdam and note #9 under 'Suggested Construction Sequence' call for the cofferdam to be cut a left in place. Please clarify which method will be required.
- Response 6) See revised Plan, Sheet 45 of 58.
- Question 7) Drawing 45 of 58, Note #1 under pre-drilling notes requires the predrilling of the two pile at the south end of the center pier. It also requires pre-drilling of the two piles at the west abutment for the pedestrian bridge if they are used. Who decides if the two piles under the west abutment of pedestrian bridge are going to be used or not? Should the predrilling for the two piles under the west abutment be carried in our bid or not?
- Response 7) See Plans, Sheet 51 of 58. Piles are proposed for the temporary bridge abutments, but the Contractor may use spread footing foundations. This change would be proposed during construction. Length of temporary piles is included in the quantity for Item 942.124 and all work required to install the piles should be included in the bid.

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| Question 8) | Under the Army Corps of Engineers General Permit, Page A00831-4, the third paragraph, states. "You must commence or be under contract to commence the work authorized herein by April 5, 2023, and complete the work by April 5, 2024. If not, you must contact the office to determine the need for further authorization before beginning or continuing activity. Will the contract be responsible for the working with the USACE to extend the time frame for this permit? |
| Response 8) | MassDOT Environmental will reauthorize the project as necessary during Construction. |
| Question 9) | Utilities are being relocated by others, from underground to overhead. Will these temporary relocations allow cranes to work at both ends of the bridge? |
| Response 9) | Utilities are not being relocated from underground to overhead. The overhead utilities are being relocated temporarily (as overhead) to the south for the bridge construction including crane work. It was anticipated that this would allow for cranes to work at both sides of the bridge. The proposed underground electric bank is for NGRID future expansion and Telecommunications will remain underground as it is now. |
| Question 10) | Will the contractor be allowed to detour traffic during crane picks for the demolition work and for the pile driving activities? |
| Response 10) | Yes – that is why the detour was established. See Scope of Work and Prosecution of Work and Provisions for Travel in Special Provisions. |

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② Addendum No. 2, March 11, 2021

ITEM 127.1 **REINFORCED CONCRETE EXCAVATION** **CUBIC YARD**

Work under this Item shall conform to the relevant provisions of Subsection 120 of the Standard Specifications and the following:

The work to be done under this item includes furnishing all labor and equipment necessary to perform demolition and removal of the existing bridge abutments, wingwalls, and pier to the limits shown on the plans. Coring through existing concrete foundations to provide holes or openings for pile driving is also included under this item.

The demolition of portions of the reinforced concrete substructure elements is included in Item 127.1 Reinforced Concrete Excavation.

Prior to the start of work, the Contractor shall locate all utilities and shall submit to the Engineer and the utility companies his proposed method of protecting them during the demolition operations. Procedure submittal shall not serve to relieve the Contractor of his responsibility to protect all utilities from damage at all times. Any damage done to utilities by the Contractor shall be immediately repaired at his expense.

The Contractor shall also prepare and submit a plan indicating his/her proposed demolition procedures and methods to be used including equipment, tools, devices, and schedule of operations to the Engineer for review. Work under this item may not commence until the Engineer has given written approval.

Temporary shielding to prevent debris from falling into the waterway below the bridge shall be paid for under Item 994.01 Temporary Protective Shielding Bridge No. W-37-015 (0AL). Any equipment, debris or excavated material that falls into the river due to the Contractor's activities shall be promptly removed by him at his expense and as directed by the Engineer.

②

EQUIPMENT

Surface preparation and concrete removal equipment will be of the following types:

- (1) Pneumatic and Power Driven Chipping Hammers in excess of thirty-five-pound class may be used for the removal of concrete where sound or unsound concrete may be encountered in the areas of known demolition. The Contractor will take care not to damage the reinforcing steel that will be re-used in the proposed construction. The Engineer may reject methods or equipment that may render the reinforcing steel unsuitable for re-use.
- (2) Grit Blasting Equipment: Grit blasting equipment will be capable of removing rust and old concrete from exposed reinforcing steel when deemed necessary.

The Engineer may reject the use of any methods or equipment that causes undue vibration or possible damage to the structure or any part of the structure during the work.

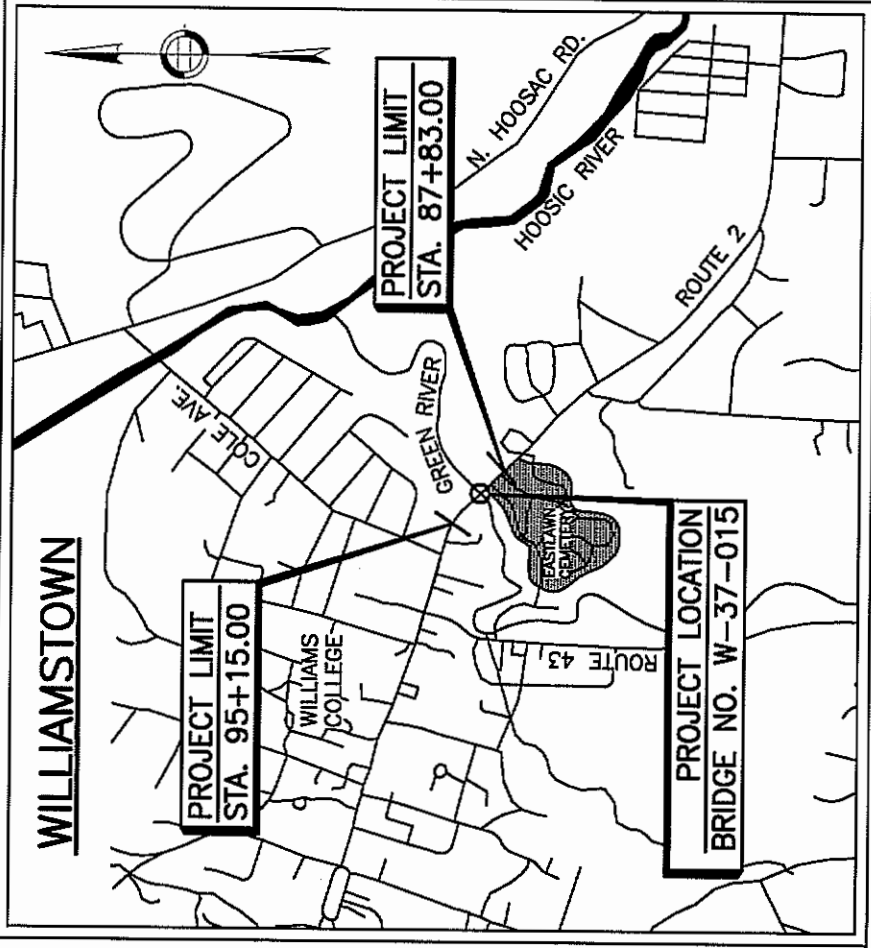
WILLIAMSTOWN
ST 2 (MAIN STREET)

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	NHP(BR-ON)-003S(205)X	26	56

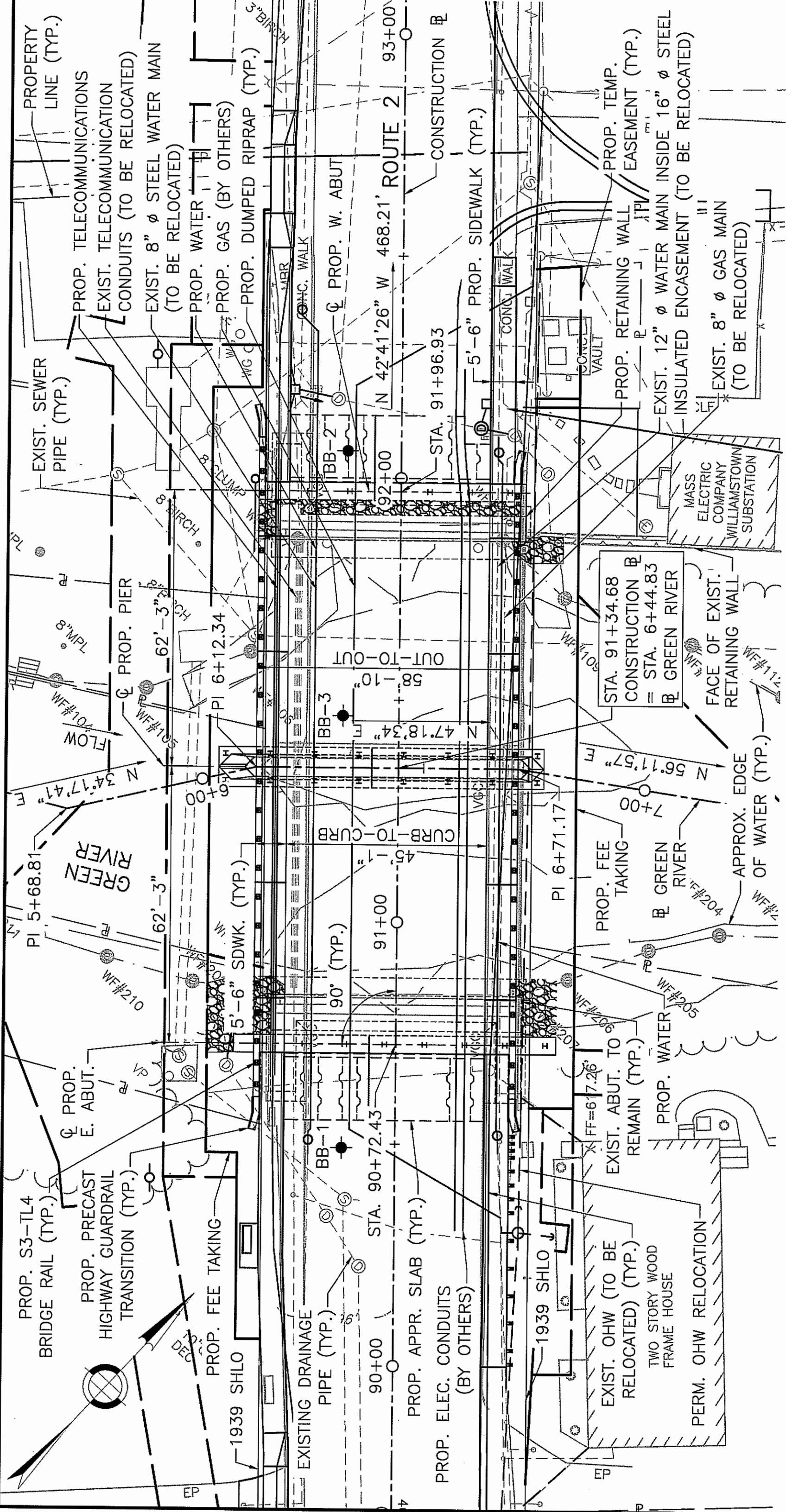
PROJECT FILE NO. 605356
GENERAL PLAN

INDEX OF SHEETS

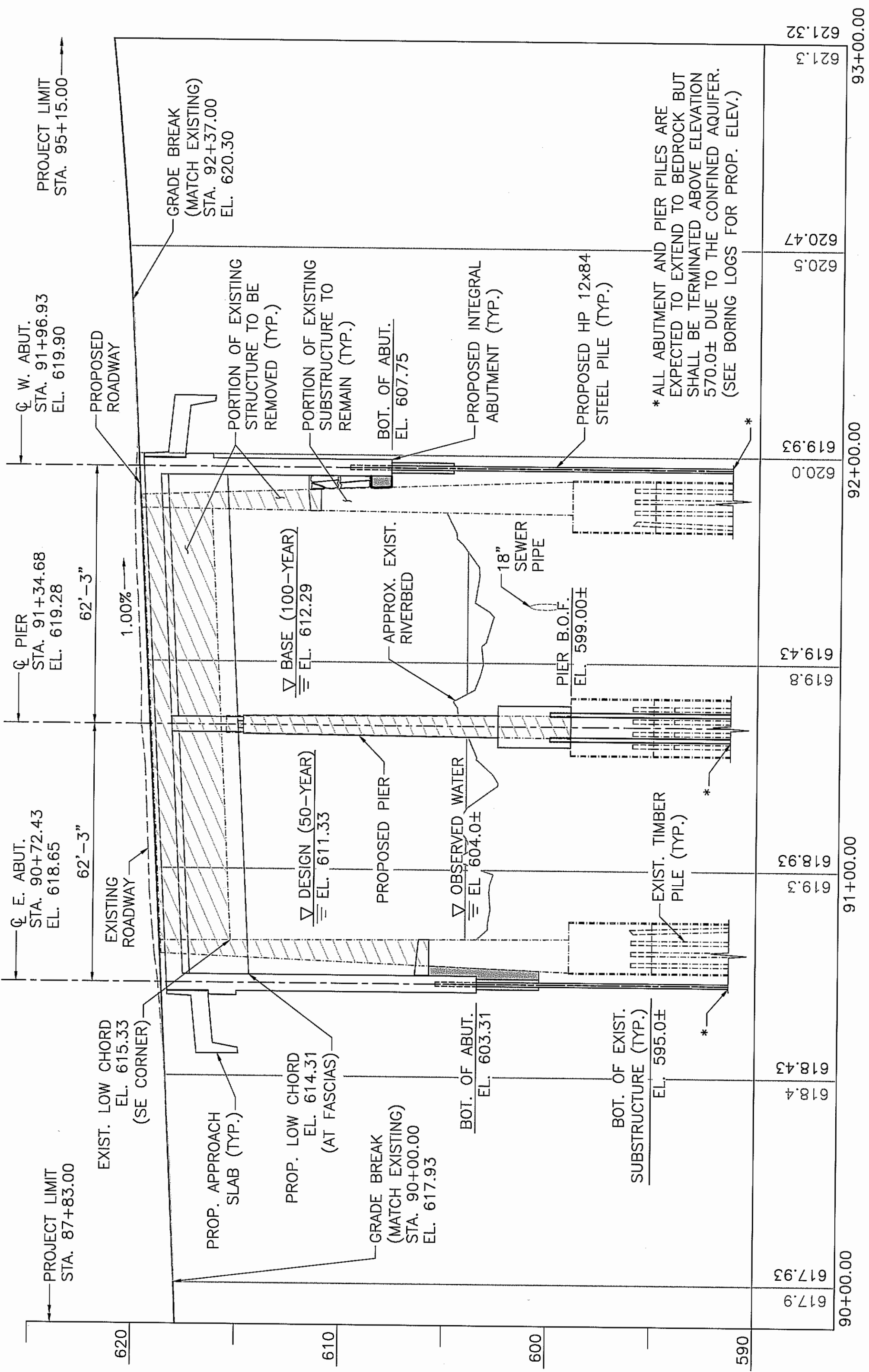
- GENERAL PLAN
- GENERAL NOTES
- BORING LOGS I
- BORING LOGS II
- BORING LOGS III
- LAYOUT & ELEVATION
- CROSS SECTIONS
- STAGE CONSTRUCTION I
- STAGE CONSTRUCTION II
- DEMO PLANS
- EAST ABUTMENT
- WEST ABUTMENT
- EAST ABUTMENT LAYOUT
- WEST ABUTMENT LAYOUT
- ABUTMENT DETAILS
- WINGWALL ELEVATIONS
- WINGWALL DETAILS
- PIER ELEVATION
- PIER DETAILS I
- PIER DETAILS II
- FRAMING PLAN
- BEAM DETAILS
- SLAB DETAILS
- APPROACH SLAB DETAILS
- UTILITY DETAILS
- TEMPORARY PEDESTRIAN BRIDGE
- HIGHWAY GUARDRAIL TRANSITION DETAILS
- S3-TL4 DETAILS
- PRECAST HIGHWAY GUARDRAIL TRANSITION



LOCUS
SCALE: 1" = 2000'

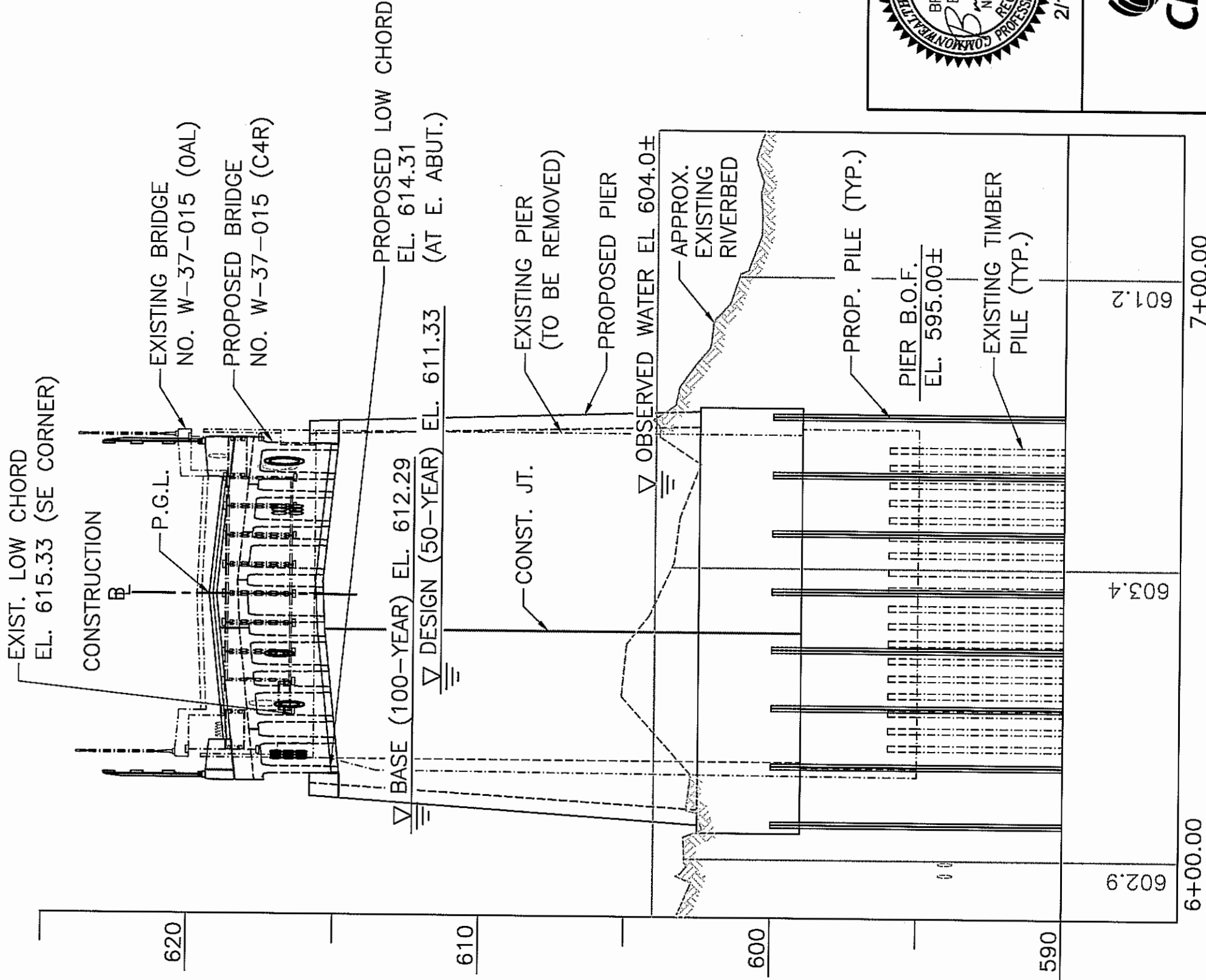


KEY PLAN
SCALE: 1" = 20'-0"



PROFILE — CONSTRUCTION B (ROUTE 2)

HORIZONTAL SCALE: 1" = 20'-0"
VERTICAL SCALE: 1/4" = 1'-0"



PROFILE — GREEN RIVER

HORIZONTAL SCALE: 1" = 20'-0"
VERTICAL SCALE: 1/4" = 1'-0"



JANUARY 23, 2021

ISSUED FOR CONSTRUCTION



PROPOSED BRIDGE

WILLIAMSTOWN

ST 2 (MAIN STREET)
OVER GREEN RIVER

MASSACHUSETTS DEPARTMENT OF TRANSPORTATION
HIGHWAY DIVISION, MASS
10 PARK PLAZA BOSTON, MASS

15
TITLE: *Stacy B. Grogan, P.E.*
Stacy B. Grogan, P.E.
CHIEF ENGINEER



CME ASSOCIATES, INC.

101 East River Drive, 1st Floor
East Hartford, CT 06108
860-290-4100

