

SUBSET #06 - FOR INFORMATION ONLY RECORD PLANS			
INDEX OF DRAWINGS			
DRAWING NUMBER	DRAWING TITLE	DRAWING NUMBER	DRAWING TITLE
INX-03	INDEX OF DRAWINGS		
FIO-01 TO FIO-23	PROJECT NO. 0131-0102		
FIO-24 TO FIO-32	PROJECT NO. 0131-0132		
FIO-33 TO FIO-35	PROJECT NO. 0025-0133		

REV	DATE	REVISION DESCRIPTION

SIGNATURE BLOCK:



DESIGNER/DRAFTER: PRP
CHECKED BY: JSP



CONNECTICUT
DEPARTMENT OF
TRANSPORTATION

PROJECT TITLE:

SOUTHINGTON REST AREA TRUCK PARKING
IMPROVEMENTS

TOWN(S):

SOUTHINGTON

DRAWING TITLE:

INDEX OF DRAWINGS

PROJECT NO.:

0131-0211

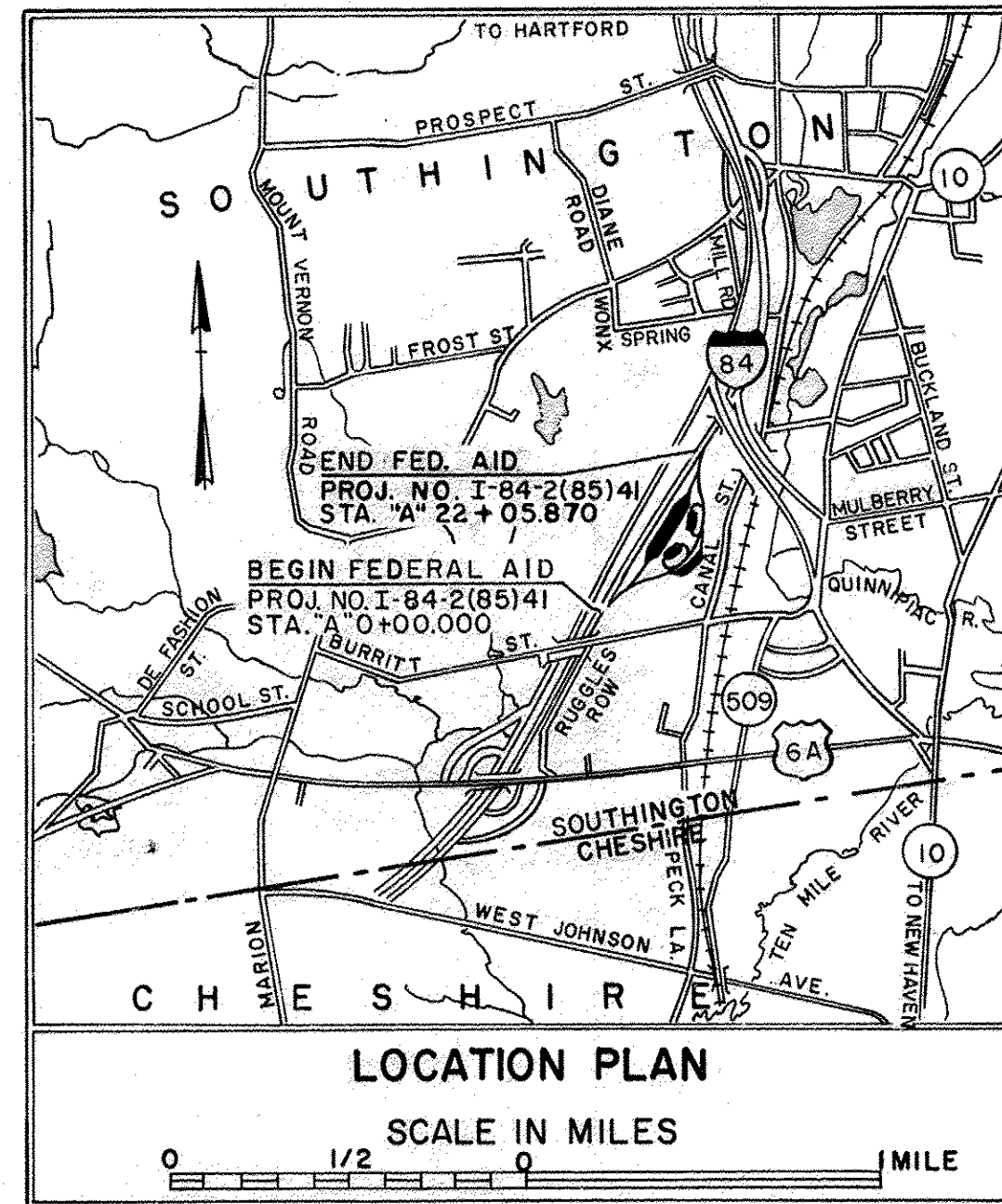
DRAWING NO.:

INX-03

SHEET NO.:

06.01

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PLOTTED DATE: 7/14/2025



CONNECTICUT STATE HIGHWAY DEPARTMENT PLAN FOR CONSTRUCTION OF REST AREA ON INTERSTATE ROUTE 84 IN THE TOWN OF SOUTHINGTON

FROM STA. A0+00.00 TO STA. A22+05.87

LENGTH 2,205.87 FT.
FEDERAL AID LENGTH = 0.00 FT.
HIGHWAY PLAN 1 IN. = 40 FT.
DESIGN PROFILE HOR. 1 IN. = 40 FT. VERT. 1 IN. = 4 FT.
SCALES CROSS SECTIONS 1 IN. = 10 FT.

OTHER SCALES AS NOTED
FEDERAL AID PROJECT NO. I-84-2(85)41
TO BE MAINTAINED BY THE STATE

STANDARD SHEETS		B. P. R.
SHEET NO.	TITLE	APPDATE
221 J	METAL & REINFORCED CONC. CULVERT END	6/20/68
222 C	CURBING	11/16/66
222 D	SIDEWALK & DRIVES	6/20/68
223 A	MANHOLE, FRAME & COVER	2/11/65
228 C	TYPE "C-L" CATCH BASIN TYPE "C-L" DROP INLET	
228 D	STEEL FRAME & GRATE TYPE A FOR TYPE "C" & "C-L" C.B.	
228 E	TYPE "C" CATCH BASIN TYPE "C" DROP INLET	2/11/65
712 C	BARRICADES, TREEWELL & ROOT PROTECTION SERVICE BRIDGES, SLOPE PAVING.	
949	PLANTING DETAILS	6/20/68
221 H	UNDERDRAINS, OUTLETS, PERFORATED PIPE & SLOTTED R.C. PIPE	4/21/70
720 A	STANDARD SIGNS FOR MAINTENANCE CONSTRUCTION & PERMIT OPERATIONS	4/21/70
220 B	PAVED DITCH	
228 F	STEEL FRAME & GRATE TYPE B FOR TYPE "C" & "C-L" C.B.	3/21/67

2-1A, 10A, 12-1, 14A, 17A, 20A, 21A, 26A } ADDED FOR
27A, 29A, 31A, 33A, 33B, 41-1, 50A, 51A } CONSTRUCTION ORDER
52A, 53A, 55A, 59A, 61A, 62A, 63A, 64A, 66A, 67A, 68A.

17B, 20B, 49A, 58A, 59B, 60A - ADDED FOR CONST. ORDER 12/1/71

2-1B, 2A, 12A, 17C, 18A, 20C, 22A, } ADDED FOR CONST. ORDER 1/18/72
23A, 24A, 25A, 27B, 58B, 79, 80 }
27C - ADDED FOR CONST. ORDER 8/18/72 JAH

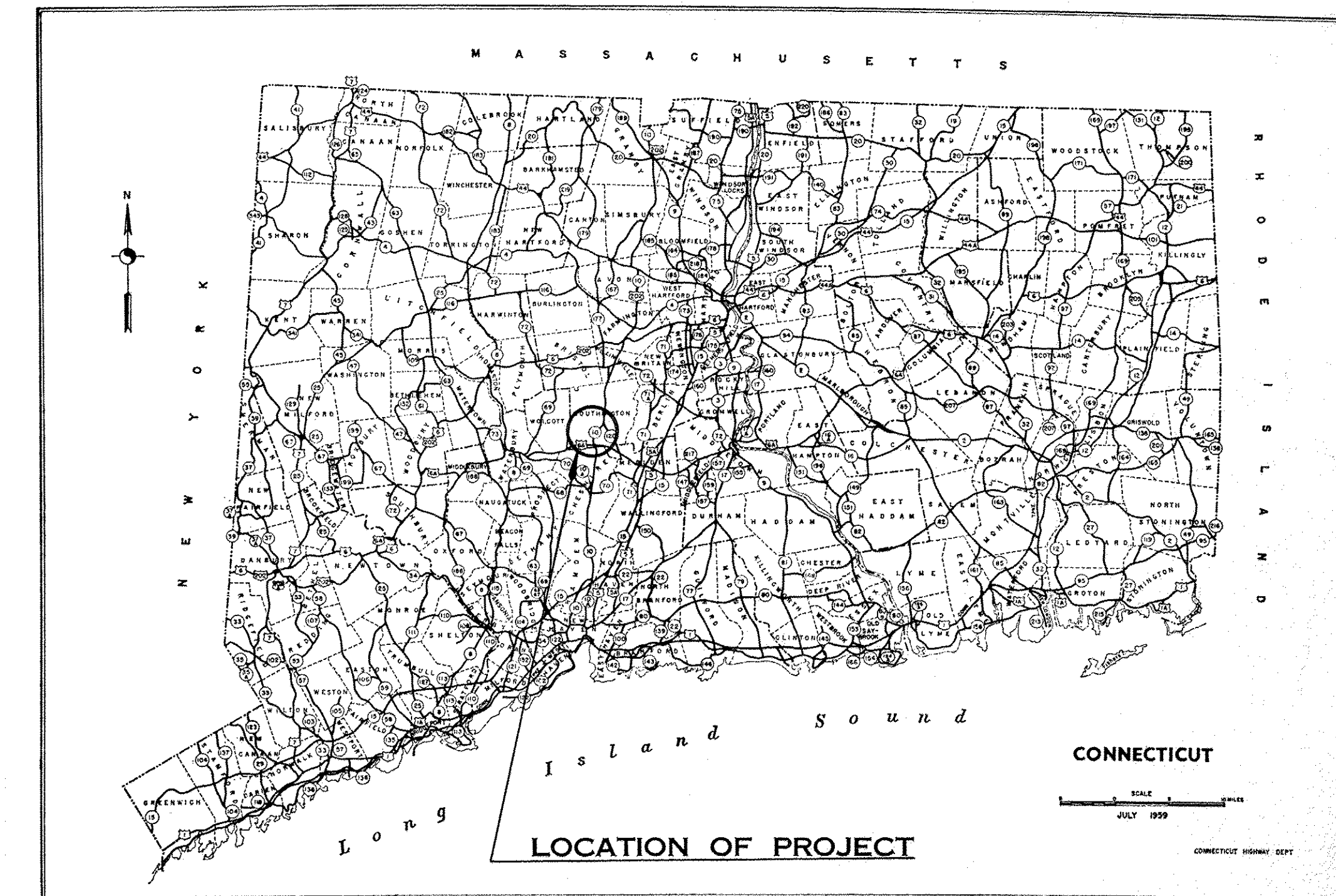
11-1, 11-2, 11-3 NEW SHEETS ADDED 11/26/74 JAH
PAVEMENT PAINTING PLANS

STANDARD CONVENTIONS

Hedge	Picket Fence	Present Culvert	N.A. (L)
Guide Rail	Board Fence	Proposed Culvert	Non Access
Curb	Retaining Wall	House	Taking Line
Stalled Weather	Open Ditch	Barn	
Stone Wall	Flowing Stream	Buildings	
Concrete Walk	Open Ditch	Swamp	
Walk - Ten, Gravel	Open Ditch	Gas Refinery	
or Earth	Telegraph, Tele-	Sum	
Phone, or Elec-	tric Light Poles	Highway Map	
Sign	Trolley Poles	C.M.D. Marker	
Present Tracks	Ledge or Rock	Present C.B.	
Proposed Tracks	Tree Hydrant	Proposed C.B.	
Taking Line	Pole M.H.	Present	
Property Line	Rivers, Lakes etc.	Travel Path	
Town Line	North Point		
Grape Arbor	(Magnetic)		
Wire Fence			

LIST OF DRAWINGS	
SHEET NO.	TITLE
1	TITLE SHEET
2-3	INDEX PLAN & PROFILES
4-7	DETAILED ESTIMATE SHEETS
8-9	TYPICAL CROSS SECTIONS
10-12	MISCELLANEOUS DETAILS
13-15	GEOMETRIC PLANS
16-18	GRADING PLANS
19-22	DRAINAGE, UTILITIES & SIGN LOCATION PLANS
23-25	GRADING & DRAINAGE PROFILES
26-27	SANITARY SEWER PROFILE & DETAILS
28-29	LANDSCAPE PLANS
30-36	ELECTRICAL PLANS & DETAILS
37	REMOVAL OF RAMPS - I-84 WB
38-48	SIGN SHEETS
49	REST ROOM BUILDING SITE PLAN
50-66	REST ROOM BUILDING PLANS & DETAILS
67-68	APPURTENANCES DETAILS
69	FEDERAL AID CONSTRUCTION SIGN
70-72	CROSS SECTIONS - ROADWAY "A" & LINE "C"
73-77	CROSS SECTIONS - RAMPS I-84 WB

Sheet No. 2-1 Added, Total Sheets = 78
79, 80 ADDED 1/18/72, TOTAL SHEETS = 80



1969 SPECIFICATIONS, FORM NO. 810 INCLUDING
SUPPLEMENT THERETO DATED JULY 1970 GOVERN
ALL ELEVATIONS ON THIS PROJECT
BASED ON U. S. C. & G. S. DATUM

TOWN NO. 131
PROJECT NO. 102

FEDERAL AID SYSTEM INTERSTATE
NON-ACCESS

NOTE I-84 Main Line Constructed
As Project 131-77
Federal Aid Project No. I-84(23)41

CONSTRUCTION STARTED SEPT. 10 1971
CONSTRUCTION COMPLETED JUNE 26 1973

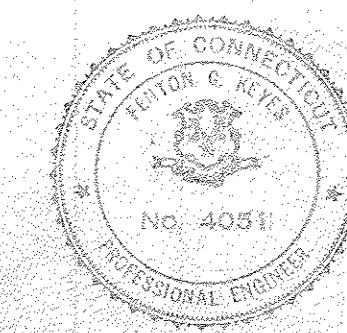
DESIGNED BY:
FENTON G. KEYES ASSOCIATES

CONN. PE. NO. 4051
DATE APRIL 29, 1970

SUBMITTED January 13 1971
Sheldon J. O'Brien
ENGINEER OF DESIGN - CONTRACTING ENGINEERS
APPROVAL
RECOMMENDED 19

CHIEF OF DESIGN
APPROVED January 19 1971
ASST. DIRECTOR OF DESIGN

U. S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
APPROVED:
DIVISION ENGINEER DATE

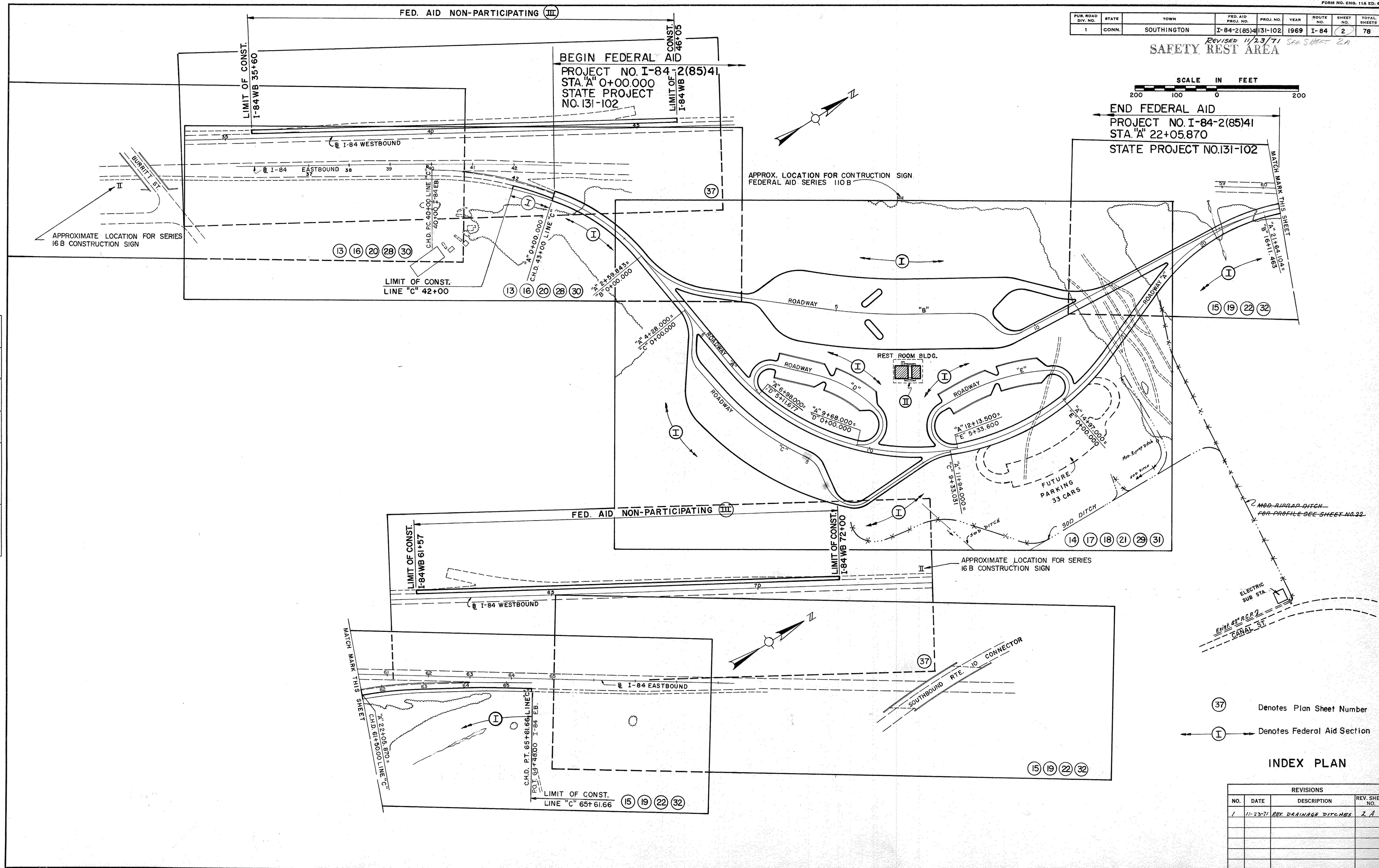


PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	2	78

REvised 11/23/71 SEE SHEET 2A
SAFETY REST AREA

SCALE IN FEET
200 100 0 200

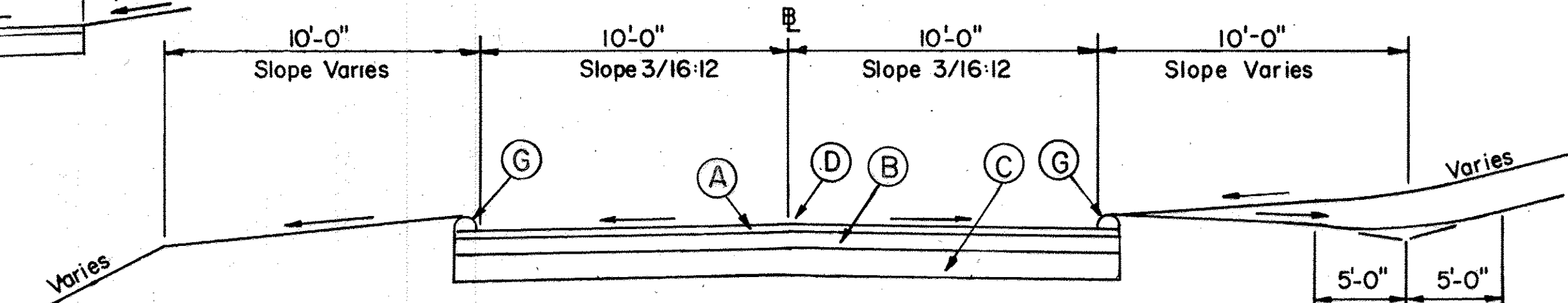
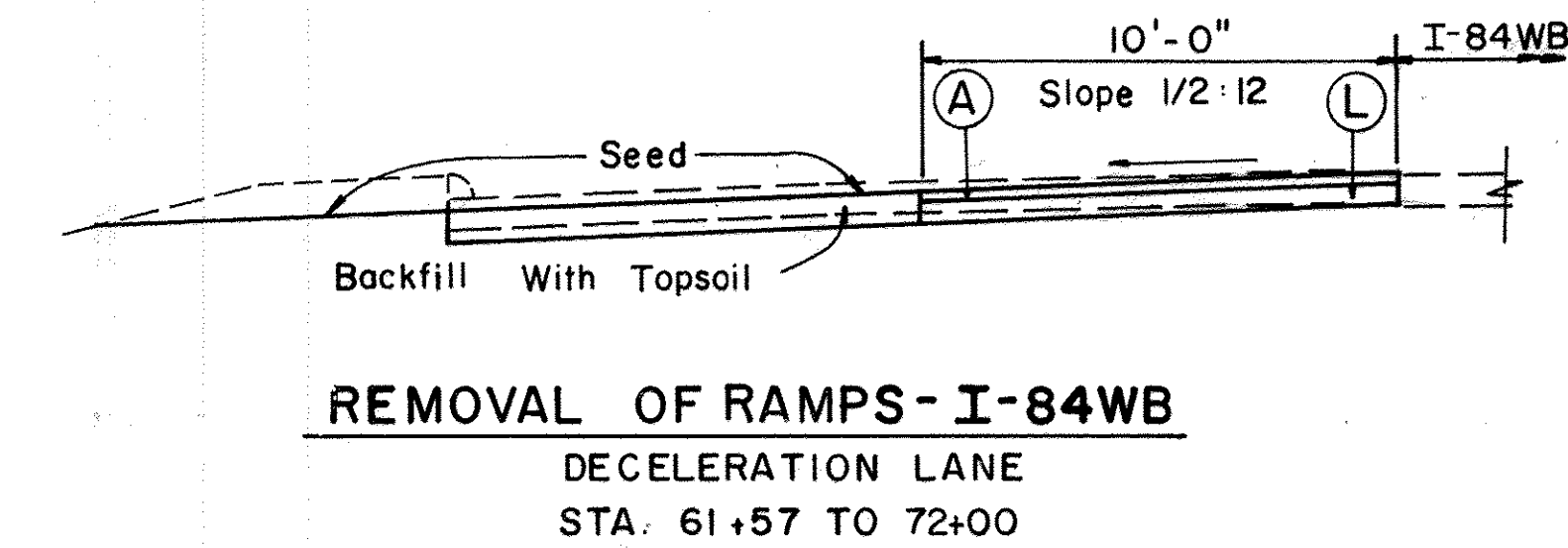
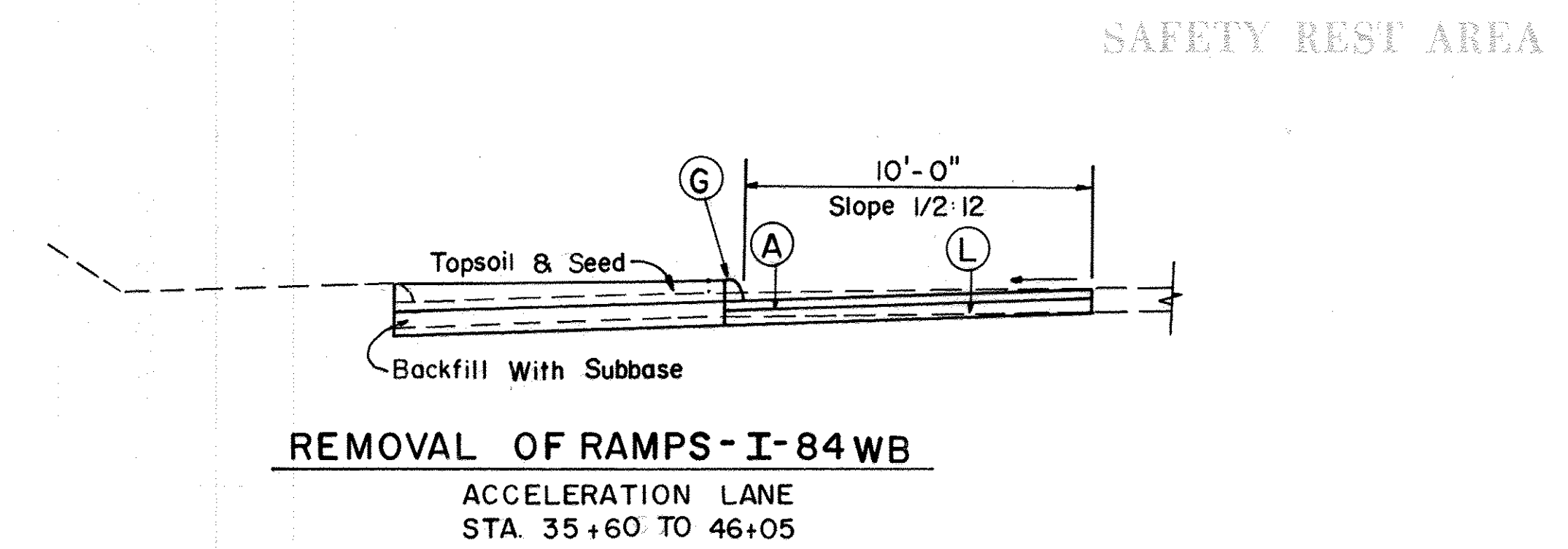
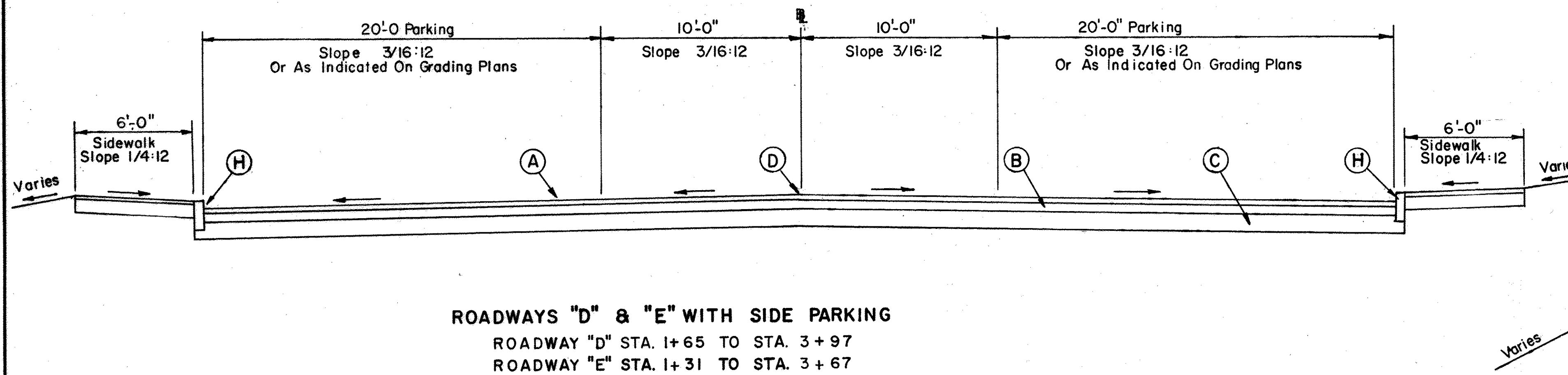
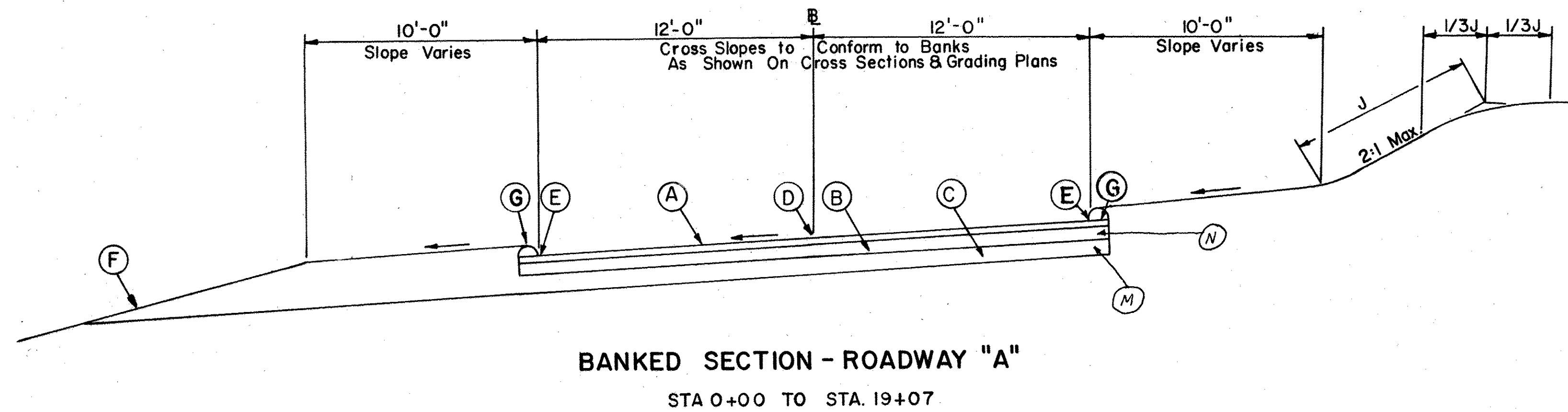
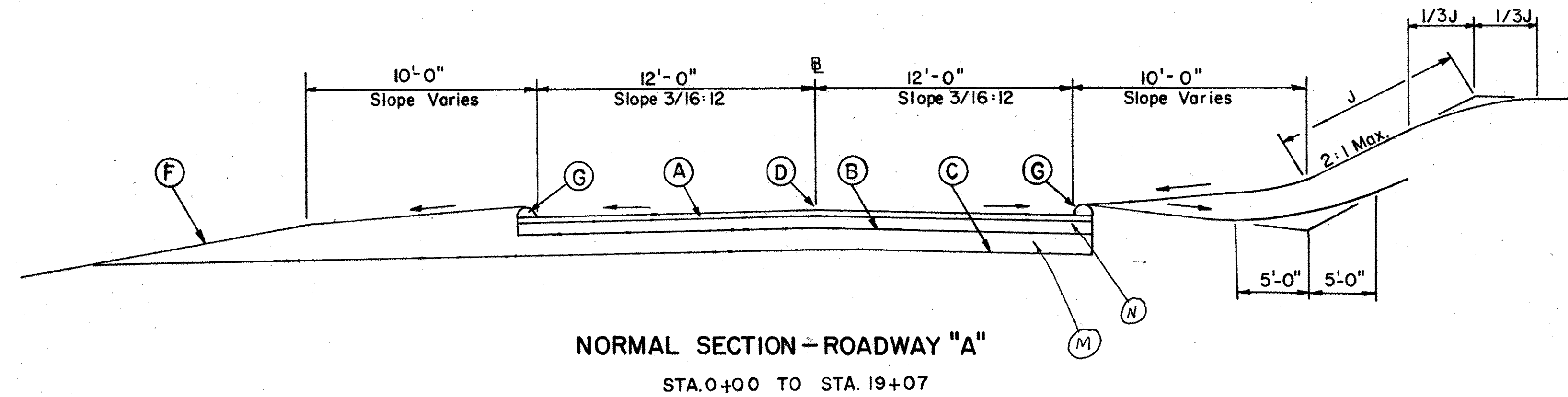
PLAN	NOTE BOOK	NO.	BY	DATE	CHECKED BY	DATE
Surveyed						
Plotted						
Traced						
Designed						
New Const. Noted						
Top. Field Chk. Noted						



TYPICAL CROSS SECTIONS

NOT TO SCALE

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)4	131-102	1969	I-84	8	78



ROADWAY "D"	STA. 0+47	TO	STA. P.C. 1+30.027
	STA. P.C. 4+05.130	TO	STA. 4+66
ROADWAY "E"	STA. 0+42	TO	STA. P.C. 0+99.600
	STA. P.C. 3+85.037	TO	STA. 4+87
ROADWAY "C"	STA. 0+07.686	TO	STA. P.C. 1+02.178

NOTE
For Legend See Sheet No. 9

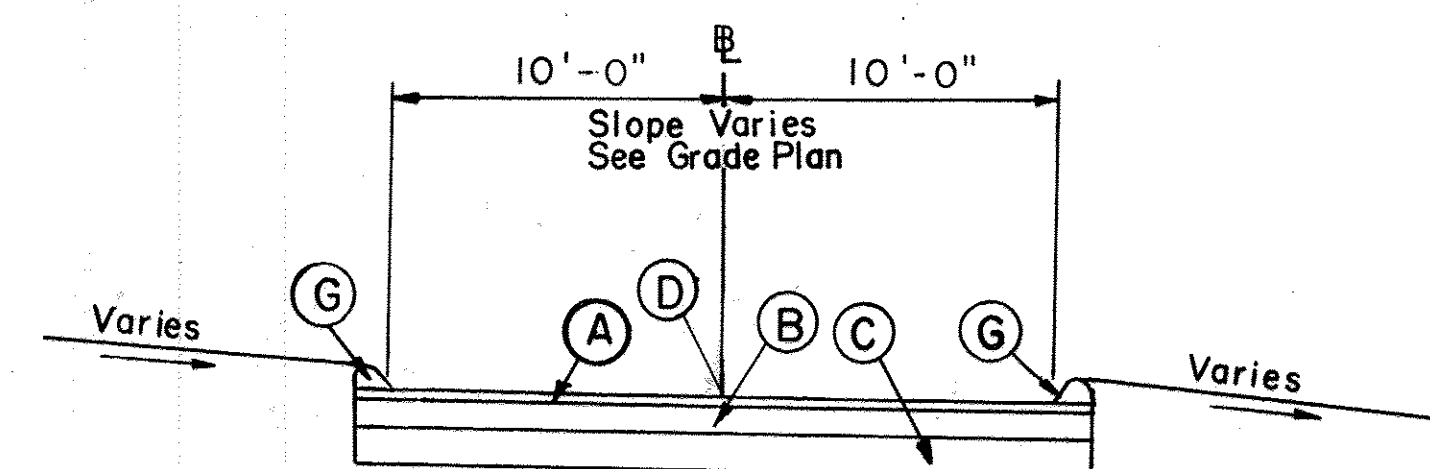
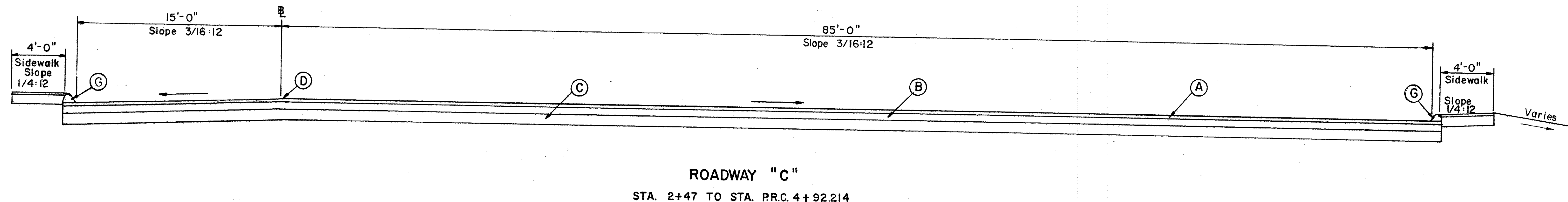
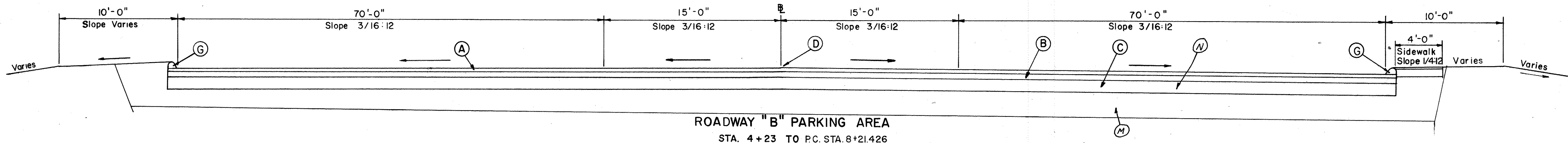
REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

TYPICAL CROSS SECTIONS

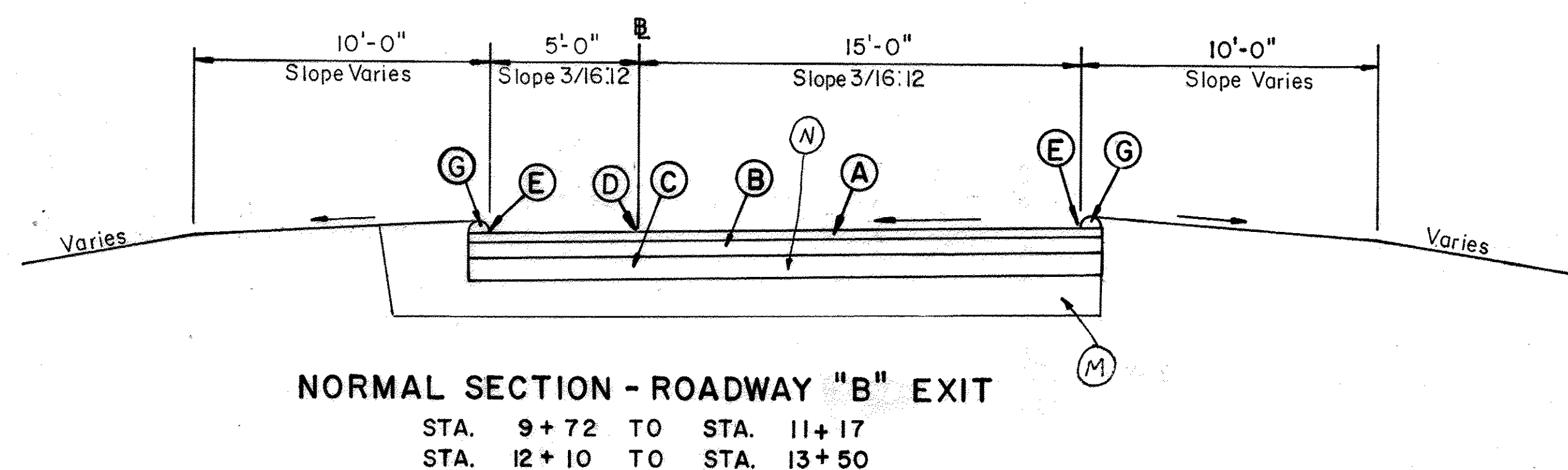
NOT TO SCALE

SAFETY REST AREA

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	9	78



NORMAL SECTION - ROADWAY "C" EXIT
STA. P.C.C. 6+44.415 TO STA. P.C. 8+62.654



(N) 6" Crusher Run Base in lieu of subbase
(C) Limits: Roadway A 0+0 - 3+90
0+0 - 1+35 B
Roadway B 1+35 - 13+50 = 19+0 A
Roadway A 19+0 - 22+05.9

LEGEND

- (A) 3" Bituminous Concrete Class I (One Course Construction)
- (B) 6" Premixed Bituminous Concrete Base
- (C) Subbase 10" In All Cuts And All Fills (See Note (K))
- (D) Point Of Application Of Grade
- (E) Point Of Application Of Bank
- (F) Slope 6:1 On Fills 0'-0" To 12'-0", 2:1 On Fills Over 12'-0", Or As Shown On Cross Sections, Grading Plans Or As Directed
- (G) Bituminous Concrete Lip Curbing
- (H) 5" Granite Stone Curbing
- (J) Slope Distance
- (K) Use 18" Subbase As Required In Areas Of Wet and/or Unstable Subgrade Encountered During Construction
- (L) 5" Calcium Chloride Stabilized Base
- (M) Free Draining Material - Depth As Directed by Engineer Same roadway limits as noted for (N)

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

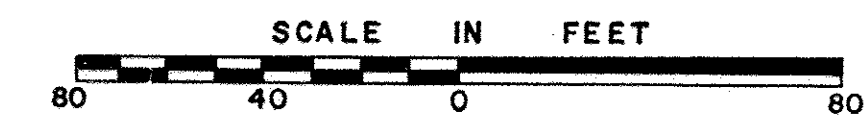
FOR INFORMATION ONLY

416-03

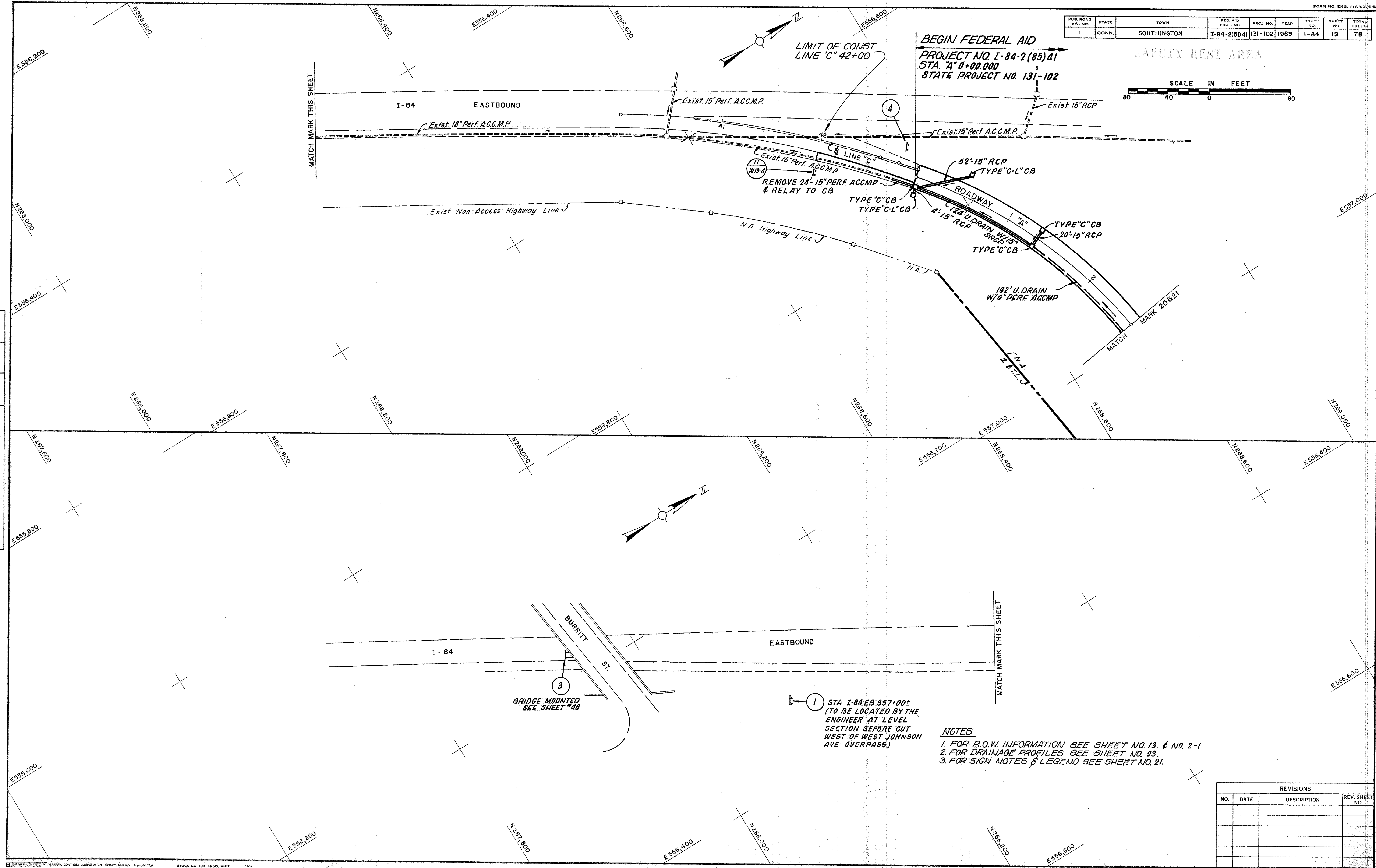
TYPICAL CROSS SECTIONS

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(5)41	131-102	1969	1-84	19	78

SAFETY REST AREA



PLAN	NOTE BOOK	NO.	BY	DATE	CHECKED BY	DATE
Surveyed						
Plotted						
Traced						
Designed						
New Const. Noted						
Top. Field Chk. Noted						



1 STA. I-84 EB 357+00±
(TO BE LOCATED BY THE
ENGINEER AT LEVEL
SECTION BEFORE CUT
WEST OF WEST JOHNSON
AVE OVERPASS)

NOTES

1. FOR R.O.W. INFORMATION SEE SHEET NO. 13. & NO. 2-1
2. FOR DRAINAGE PROFILES SEE SHEET NO. 23.
3. FOR SIGN NOTES & LEGEND SEE SHEET NO. 21.

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

FOR INFORMATION ONLY

416-03 DRAINAGE, UTILITIES & SIGN LOCATION PLAN

SAFETY REST AREA
REVISED 11/9/71

SCALE IN FEET

A horizontal scale bar with alternating black and white segments. It is marked with the numbers 80, 40, 0, and 80 from left to right. Above the bar, the text "SCALE IN FEET" is written. There are small circles or marks above the 80 and 40 markings on the left side.

Runway	Altitude	Length	Width	Surface	Remarks
Runway "D"	(A) STA. 1+56 - 2+61	61	111.0	LF	
	(B) STA. 1+62 - 2+69	67	87.0	LF	
	(C) STA. 2+81 - 3+98	87	110.0	LF	
	(D) STA. 2+97 - 3+18	81	94.0	LF	
	(E) STA. 1+28 - 2+33	45	95.0	LF	
Runway "E"	(F) STA. 1+31 - 2+27	66	89.0	LF	
	(G) STA. 2+54 - 3+77	84	121.0	LF	
	(H) STA. 2+62 - 3+67	65	100.0	LF	
	(I) STA. 2+62 - 3+67	65	100.0	LF	

LEGEND

- WATER BUBBLER WITH SILL COCK
 —W— WATER LINE
 —S— SANITARY LINE
 C.I.M.J. - CAST IRON MECHANICAL JOINT
 S.R.C.P. - SLOTTED REINFORCED CONCRETE PIPE
 R.C.C.E. - REINFORCED CONCRETE CULVERT END

NOTES

1. FOR R.O.W. INFORMATION, SEE SHEET NO. 14 & NO. 2-1
2. FOR DRAINAGE PROFILES, SEE SHEET NOS. 23, 24 & 25
3. WATER SERVICES TO OUTSIDE WATER BUBBLERS SHALL BE PLASTIC PIPE, MINIMUM 4" DEPTH OF COVER IS REQUIRED OVER WATER LINE PIPING & TUBING.
4. FOR DETAILS OF WATER BUBBLER, SEE SHEET NO. 10
5. FOR DETAILS OF THRUST BLOCK, SEE SHEET NO. 11

[illegible]

REVISED 10/5/71 SEE SLT 21A

RECEIVED 10/3/11
CAMPBELL COUNTY DISTRICT ATTORNEY

SCALE IN FEET

A horizontal scale bar with alternating black and white segments. It is marked with the numbers 80, 40, and 0 from left to right. The word "SCALE" is centered above the bar, and "IN FEET" is to its right.

CONN. LIGHT & POWER CO.
TRANSMISSION LINES.

PLAN		BY	DATE	CHECKED BY	DATE
Surveyed					
Plotted					
Traced					
Designed					
Noted					
Top. Field Chk. Noted					

ENTRANCE SIGN
—SEE DETAILS
SHEET NO.67

~~SANITARY
DUMPING
FACILITY~~

3
ROADWAY

~~SANITARY DUMPING~~
~~FACILITY~~

FUTURE PARKING FOR 33 CARS

EXAMPLE

 LOCATION & TABULATION NUMBER

 LOCATION NUMBER
TABULATION NUMBER

1. FINAL LOCATION OF REGULATORY & WARNING SIGNS TO BE AS DIRECTED BY THE ENGINEER.
2. THE LOCATION OF ALL SIGN FOUNDATION HOLES, NOT TO CONFLICT WITH UNDERGROUND UTILITIES, SHALL BE APPROVED BY THE ENGINEER.

[illegible]

FOR INFORMATION ONLY

SIGN LOCATION PLAN

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	22	78

REVISED 11/19/71 SEE SHEET 22A

SAFETY REST AREA

SCALE IN FEET

23
M6-3
M1-40
INSTALLED

NOTES

1. FOR ROW INFORMATION SEE SHEET NO. 15 & NO. 2-1
2. FOR DRAINAGE PROFILES SEE SHEET NO. 23 & 24
3. FOR SIGN NOTES & LEGEND SEE SHEET NO. 21

END FEDERAL AID
PROJECT NO. I-84-2(85)41
STA. "A" 22+05.870
STATE PROJECT NO. 131-102

Exist. 22"x13"
ACCMP Arch

REMOVE E.W. #6' OF
22"x13" ACCMP
ARCH

TYPE "C" CB
INSTALLED

REMOVE EXIST. CB
REMOVE 50'-15" PERE. ACCMP
& RELAY TO CB

TYPE "C" CB
(MOD) INSTALLED

LIMIT OF CONST.
LINE "C" 65+61.66

Exist. N.A. Highway Line

(N.A.)
T.L.V.

21
M1-4A
M4-10
INSTALLED

20
R1-2
INSTALLED

148'-30" RCP

R.C.C.E.
INV. 187.8

MOD. RIPRAP DITCH
2' WIDE, 2' DEEP

LIMITS OF
DRAINAGE R.O.W.

170 MOD.
RIPRAP

Exist. 22" R.C.P. 2

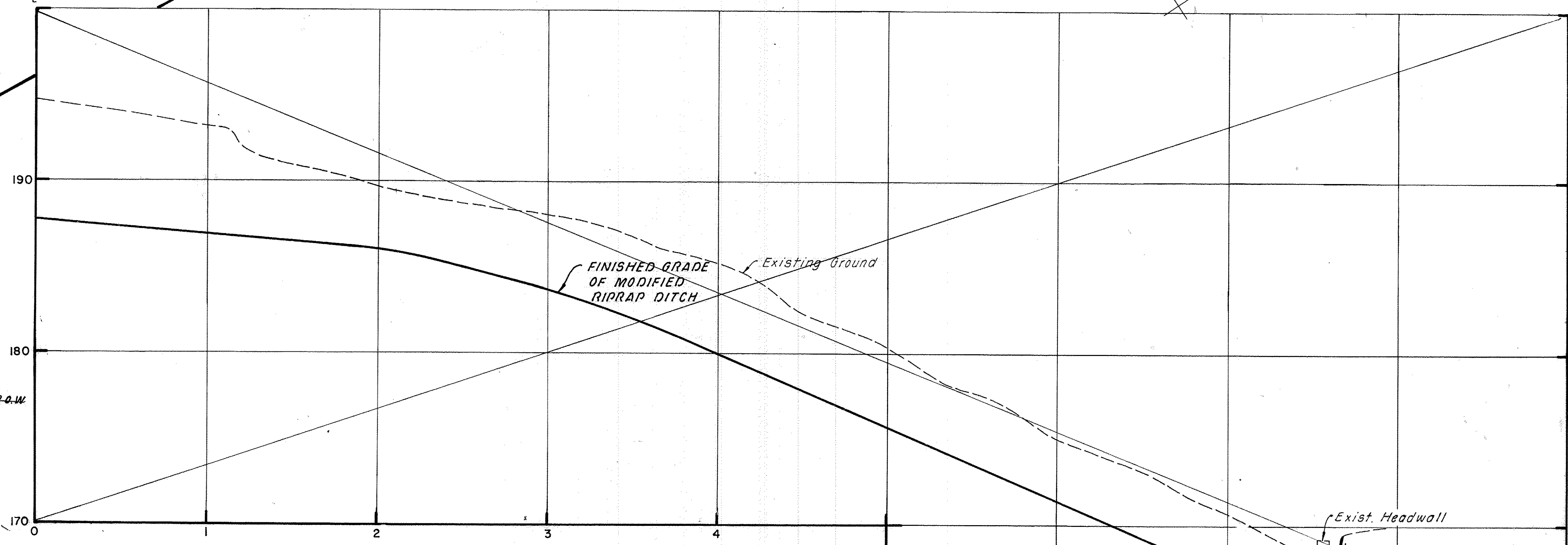
Street Line

INSERT "A"

CANAL

SWD DITCH
2' WIDE, 1' DEEP

MATCH MARK
(INSERT "A")



PROFILE OF MODIFIED RIPRAP DITCH

HOR. 1" = 40'
SCALE VERT. 1" = 4'

Exist. Headwall
MOD. RIPRAP DITCH
3' WIDE BASE, 4' HEIGHT

REVISIONS				REV. SHEET NO.
NO.	DATE	DESCRIPTION		
1	11/11/71	REV. DRAINAGE		22A

PLAN	NO.	BY	DATE	CHECKED BY	DATE
Surveyed					
Plotted					
Traced					
Designed					
Reviewed					
Top. Field Ck. Made					

MATCH MARK THIS SHEET
185

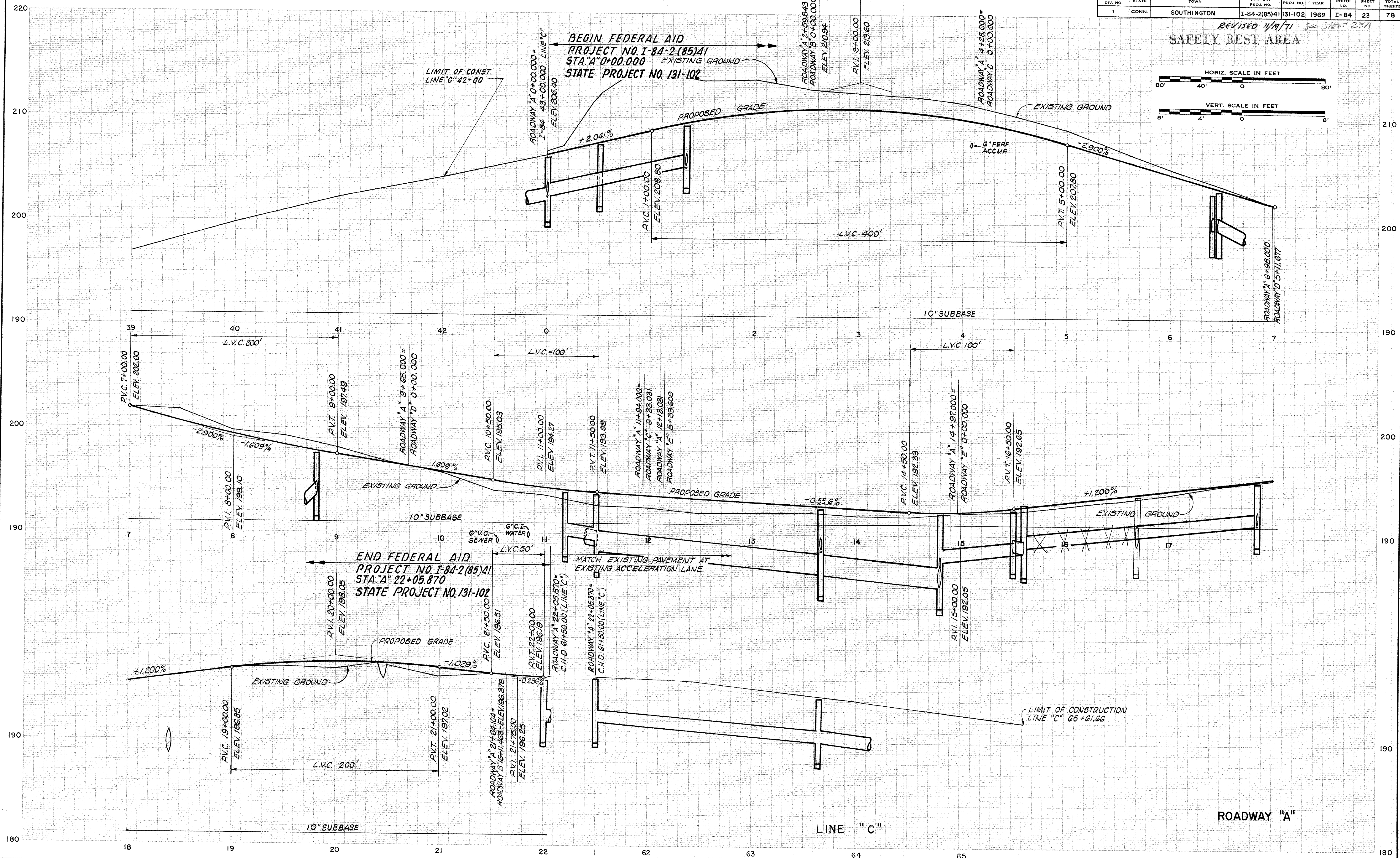
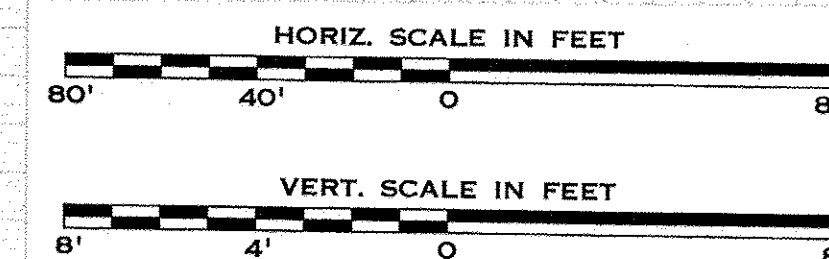
THE CONN. LIGHT
& POWER CO.
Substation

Brick Bldg.

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	23	78

REVISED 11/19/71 SEE SHEET 23A

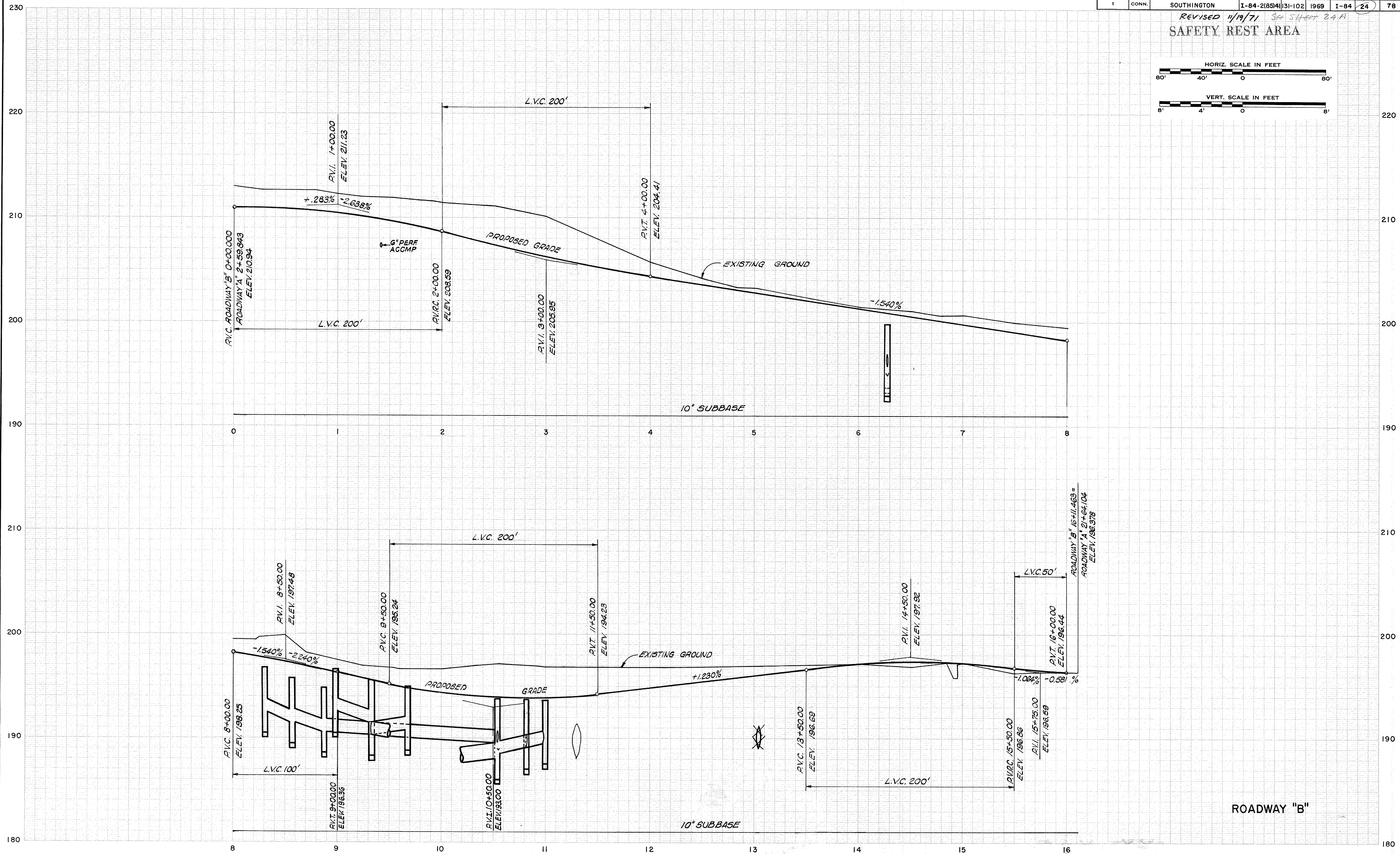
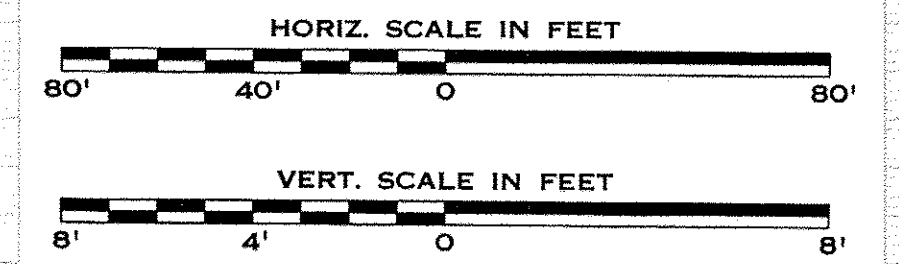
SAFETY REST AREA



PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	31-102	1969	I-84	24	78

REVISED 11/19/71 SEE SHEET 24A

SAFETY REST AREA



PROFILE	NO.	DATE	CHECKED BY	DATE
Surveyed				
Plotted				
Traced				
New Const. Noted				
Top. Field Chk. noted				

HORIZ. SCALE IN FEET

80' 40' 0

VERT. SCALE IN FEET

8' 4' 0

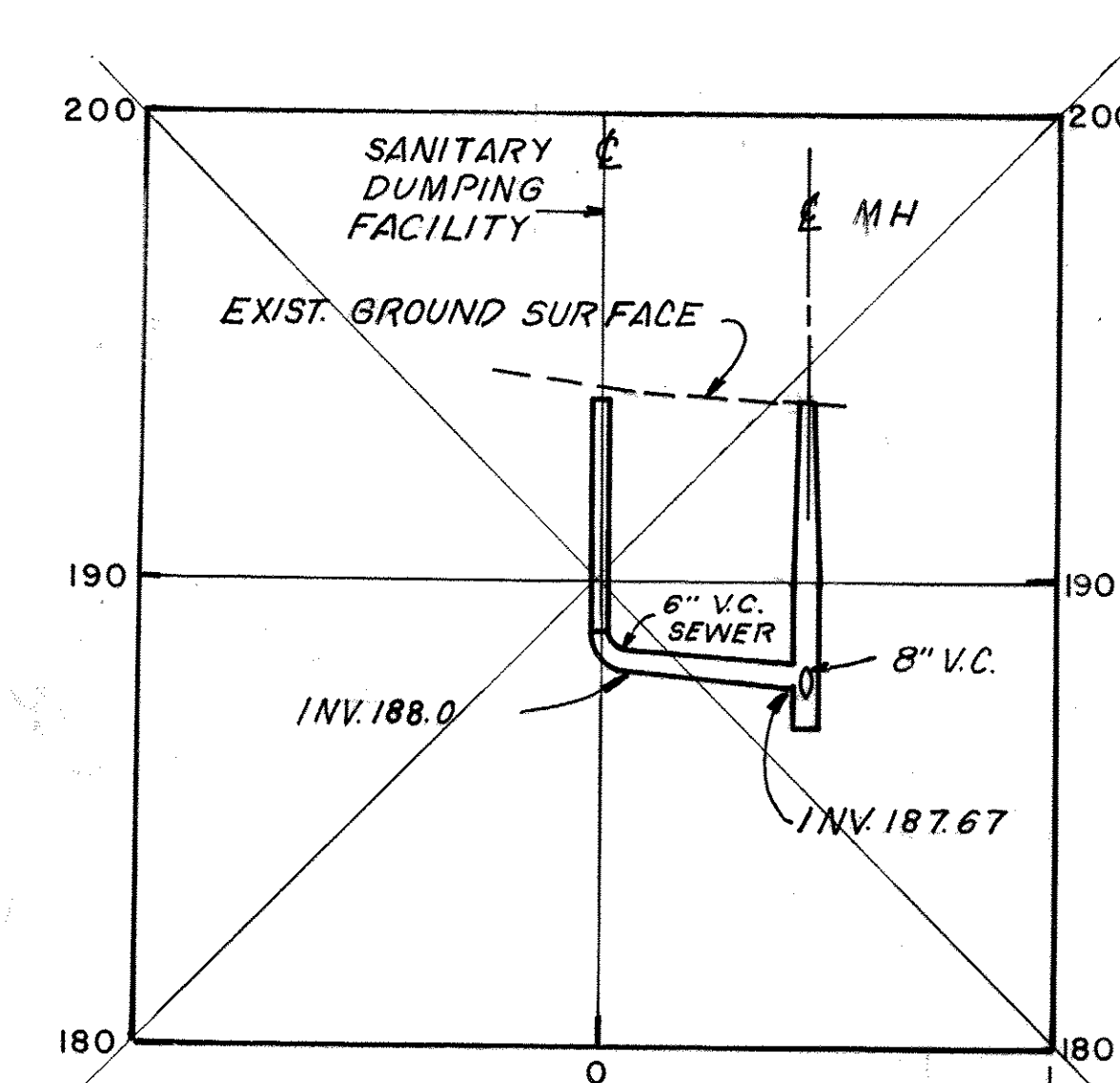


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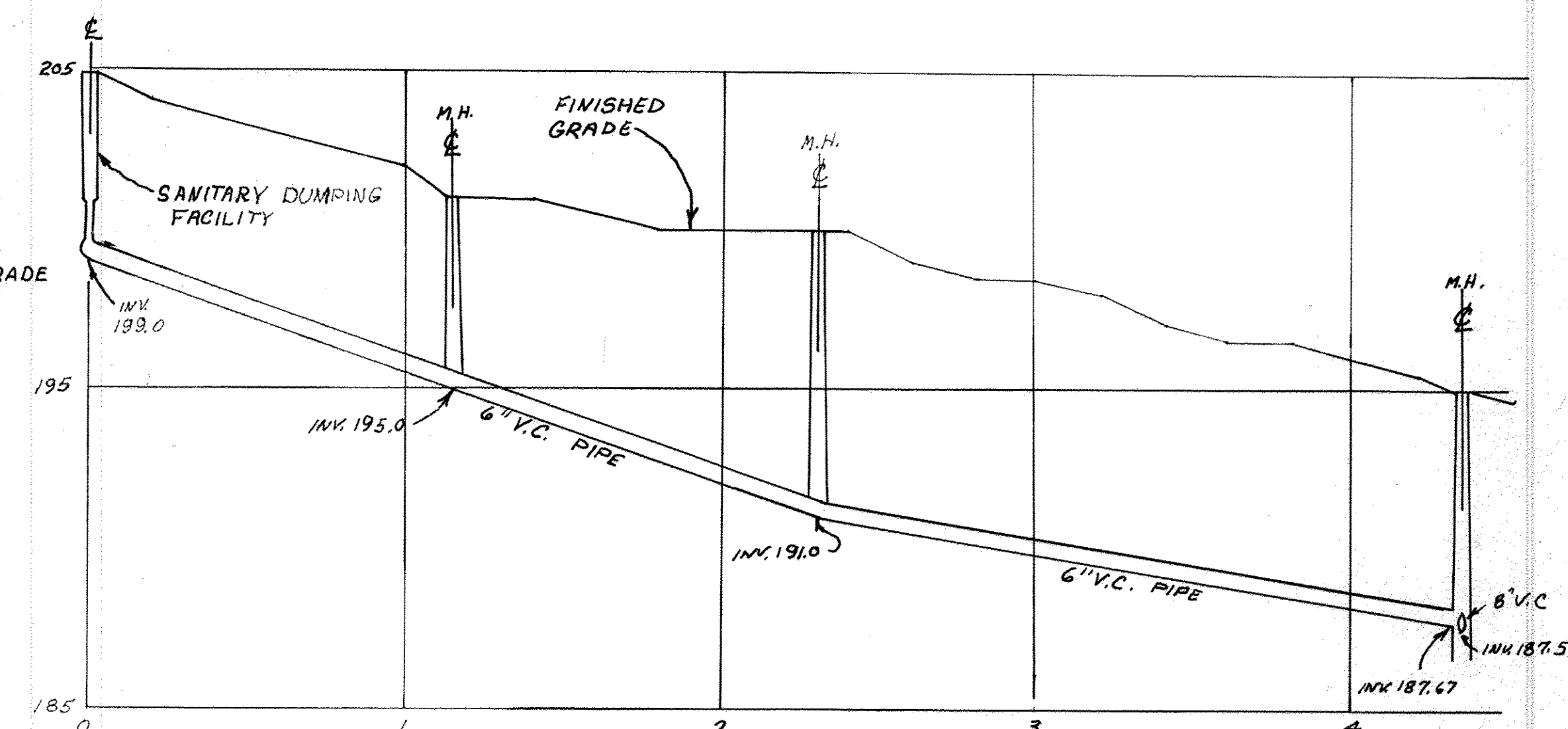


SANITARY DUMPING FACILITY DETAILS

SCALE: 1" = 1'-0"

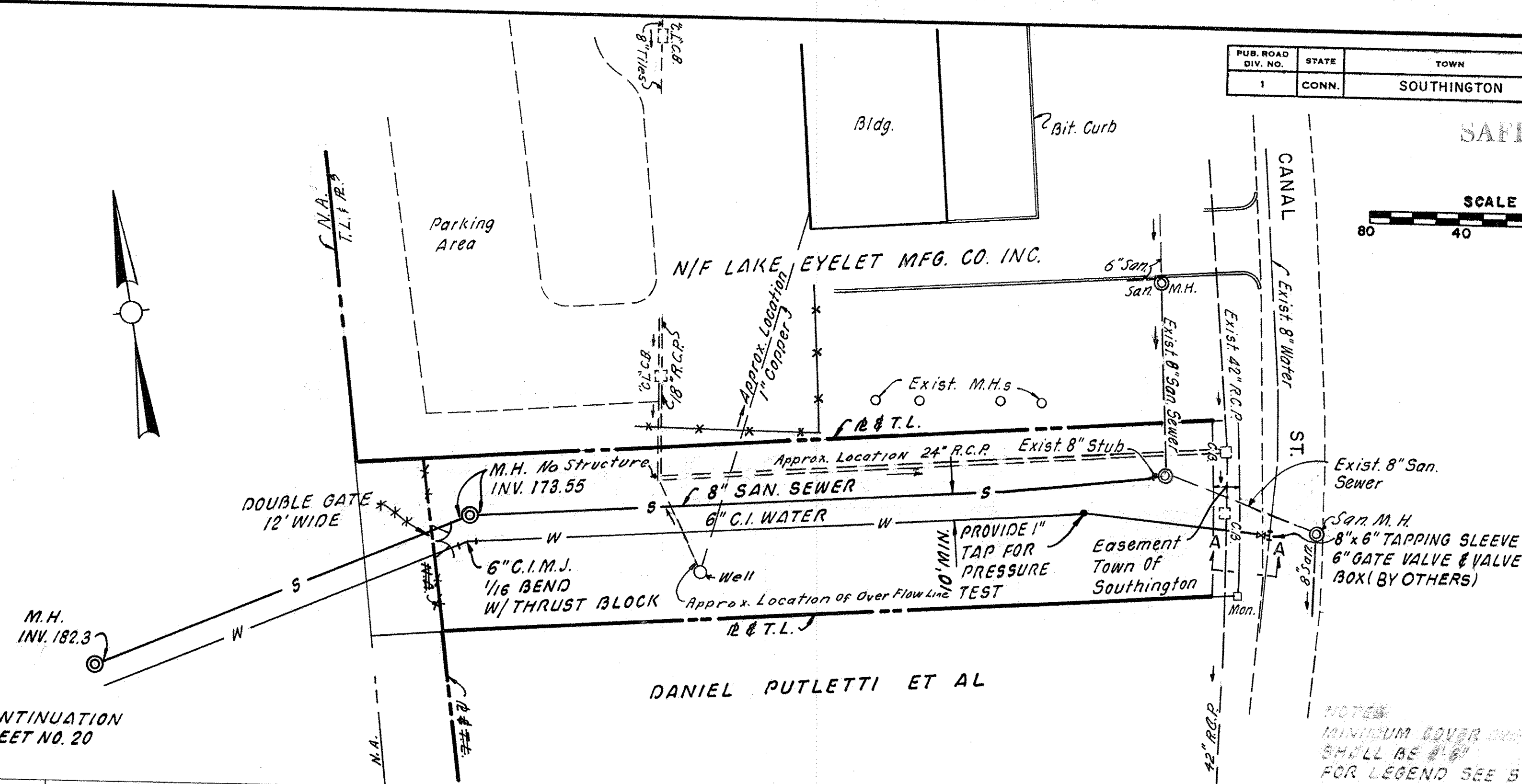


PROFILE 6" SANITARY SEWER (BELOW)
(FROM SANITARY DUMPING FACILITY TO NEW MANHOLE)
SCALE: HOR. 1" = 40'
VERT 1" = 4'



REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	10/5/71	RELOCATE SAN DUMP FAC. PROFILE	26A

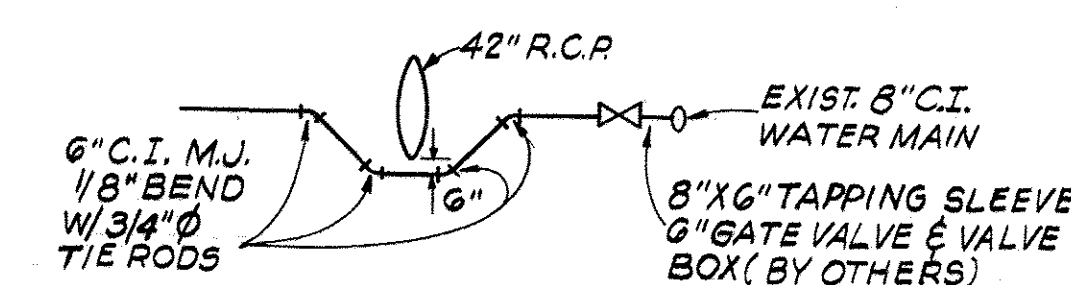
REVISED 10/5/71 SEE SH-27A
SAFETY REST AREA



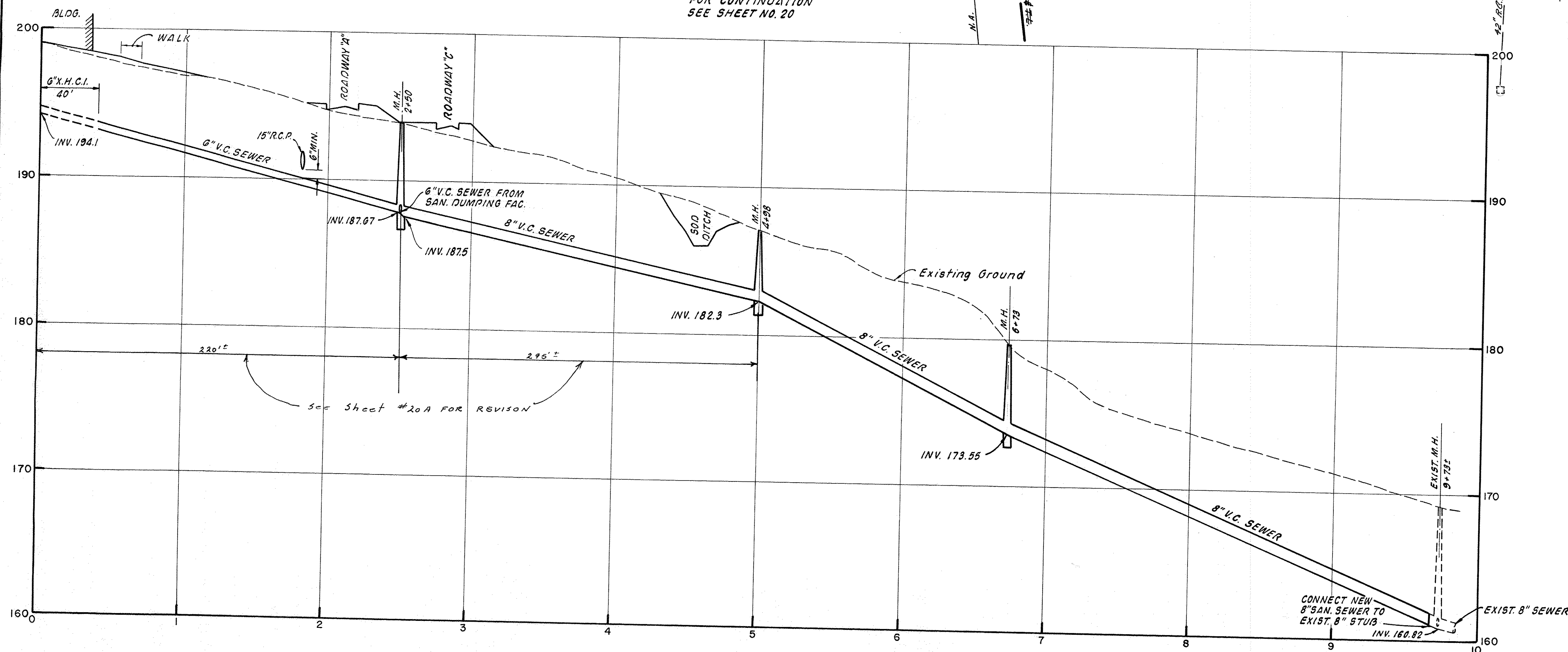
FOR CONTINUATION
SEE SHEET NO. 20

DANIEL PUTLETTI ET AL

NOTES:
MINIMUM COVER OVER WATER MAIN
SHALL BE 2'-0"
FOR LEGEND SEE S. SET NO. 20.



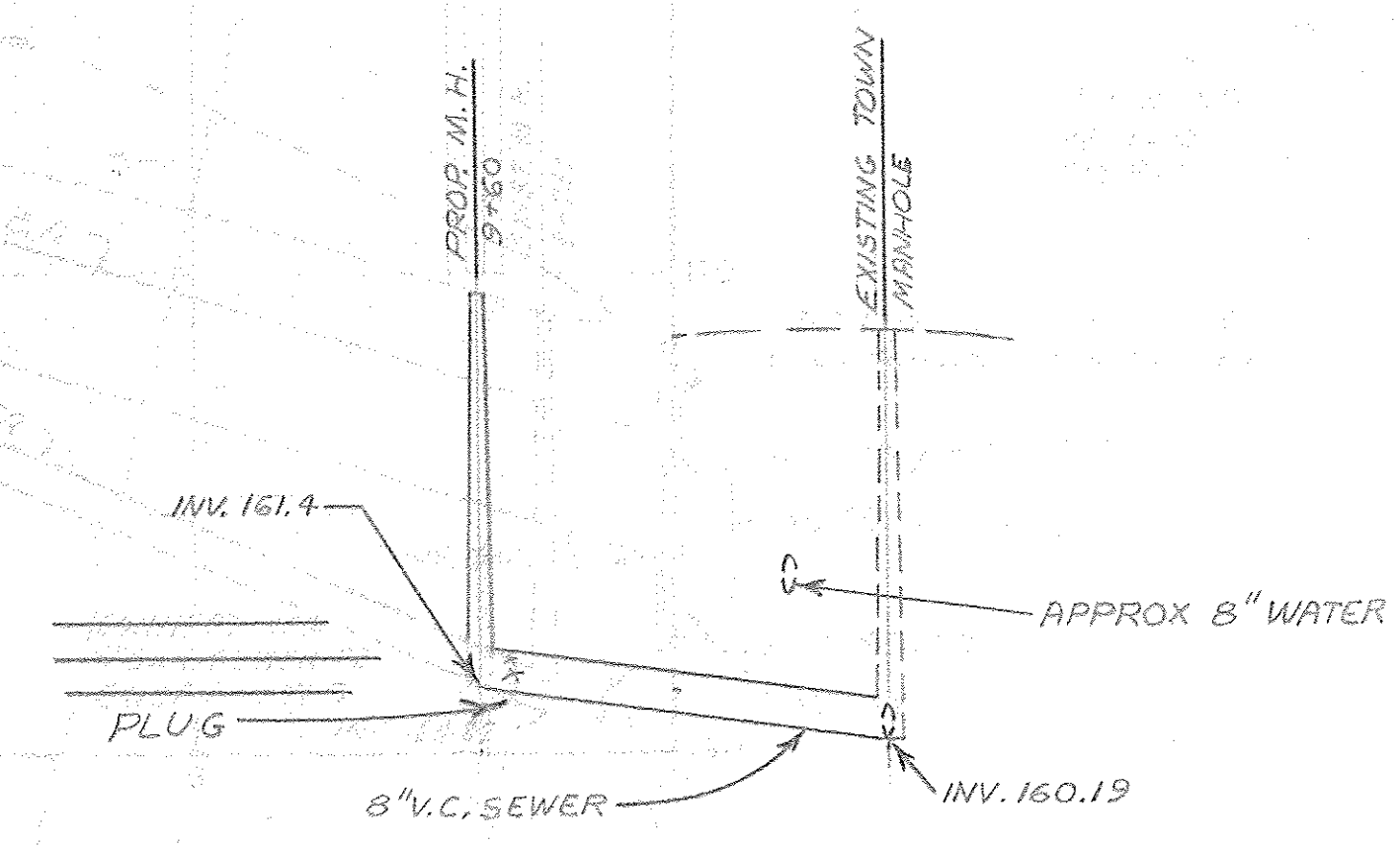
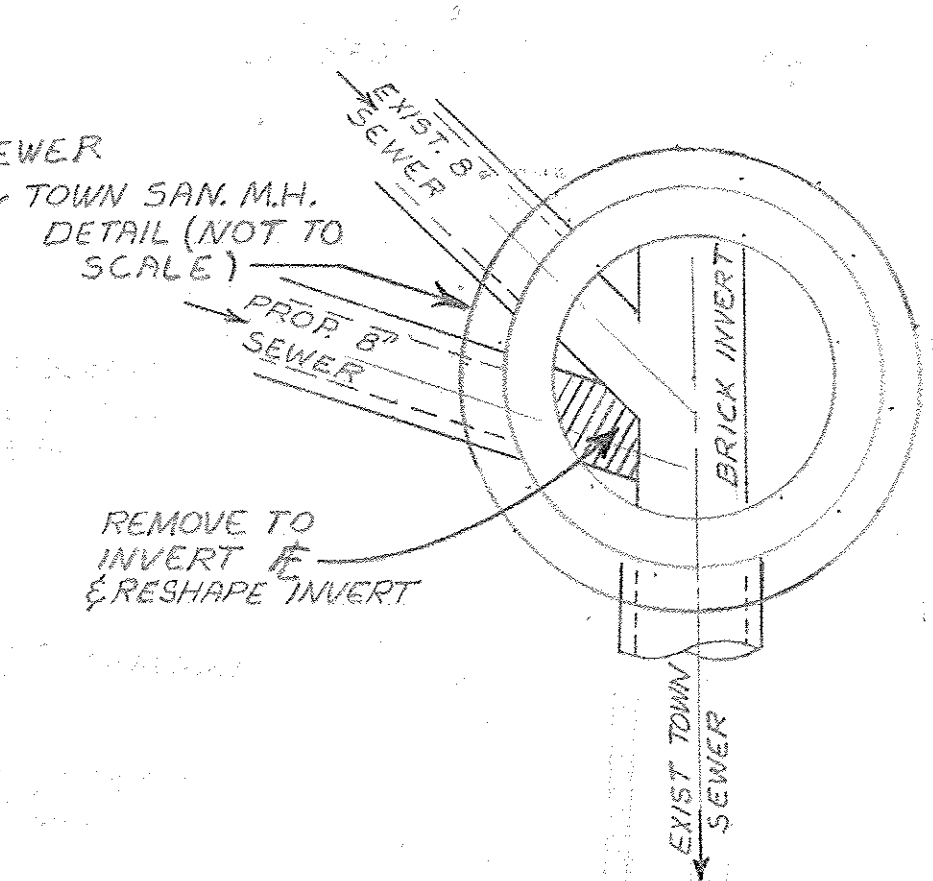
SECTION A-A
NOT TO SCALE



PROFILE 8" SANITARY SEWER
(FROM REST AREA BLDG. TO EXIST. MANHOLE)
SCALE: HORIZ. 1" = 40'
VERT. 1" = 4'

REVISONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	10-5-71	CHANGE N.A. LING CHANGE IN PROFILE	27A

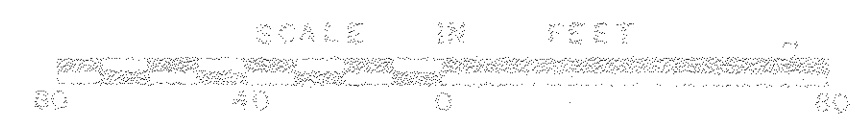
DATE	BY	CHECKED	DATE
10/1/81	10/1/81	10/1/81	10/1/81



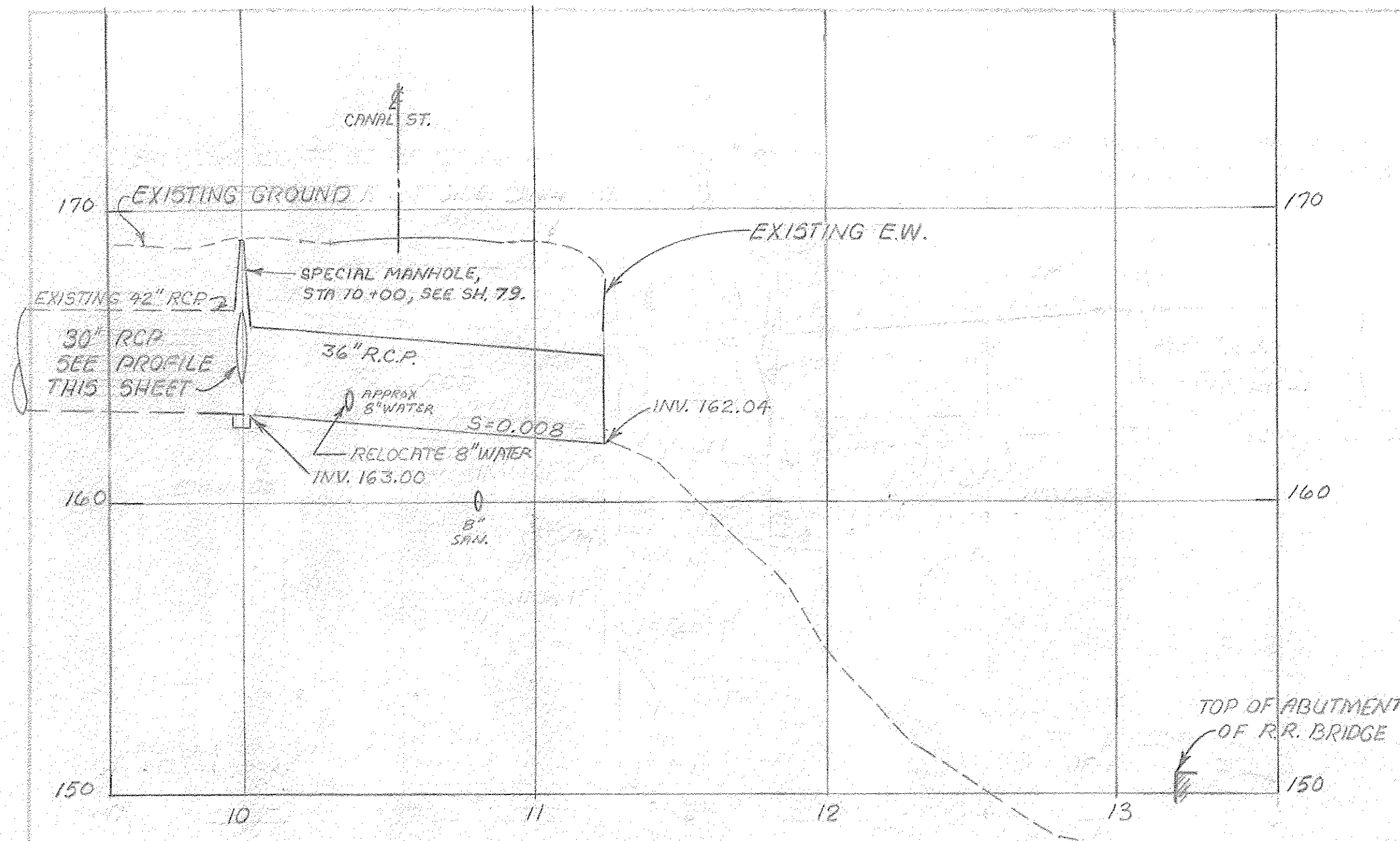
PROFILE 8" SANITARY SEWER & 4" STORM DRAIN
 (SEE SHEET NO. 208 FOR CONTINUATION)

3 8/18/81 CONNECT 8" SEWER TO M.H. IN CAVAL STREET 27C

REVISED 8/18/72 SEE SHEET 27 C



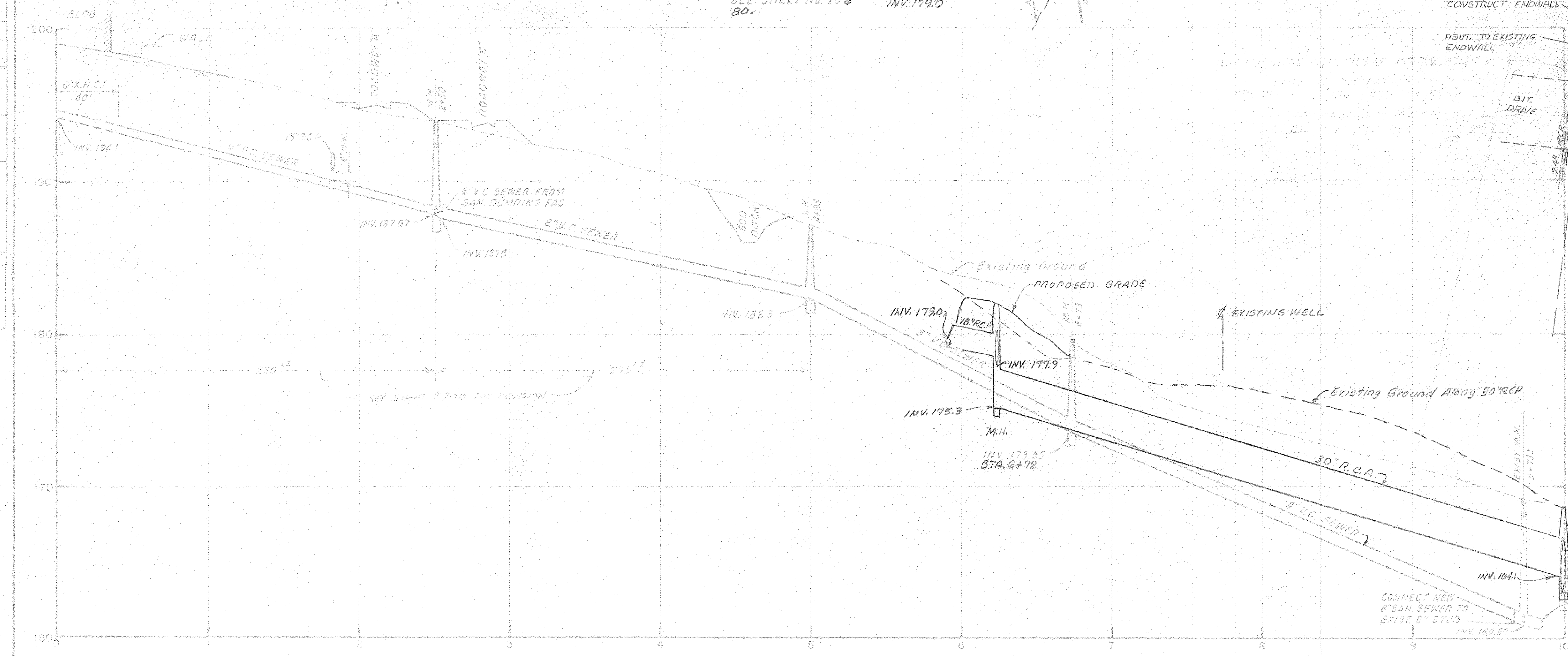
NOTES:
MINIMUM COVER OVER WATER MAIN
SHALL BE 4'-6"
FOR LEGEND SEE SHEET NO 20.



PROFILE 36" STORM DRAIN AT CANAL STREET

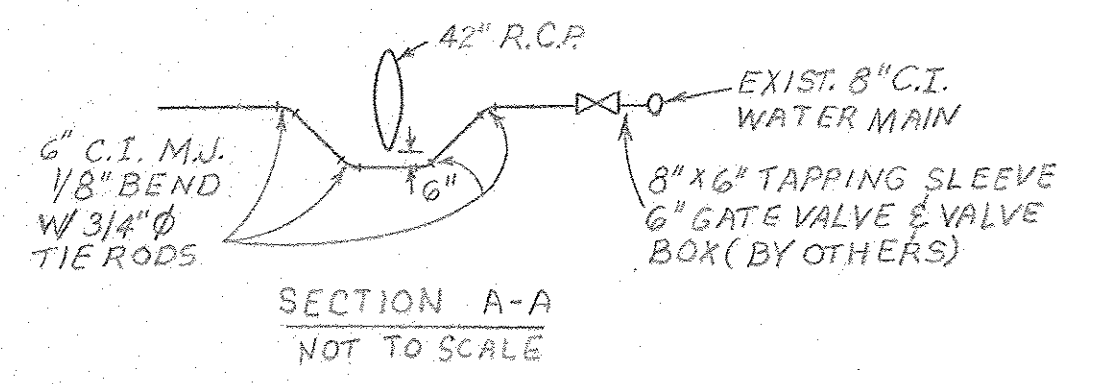


FOR CONTINUATION
SEE SHEET NO. 20 &
80.



PROFILE 8" SANITARY SEWER & 30" STORM DRAIN
(FROM REST AREA BLDG. TO EXIST. MANHOLE CANAL STREET)

HORIZ. 1" = 40'
VERT. 1" = 4'



SECTION A-A
NOT TO SCALE

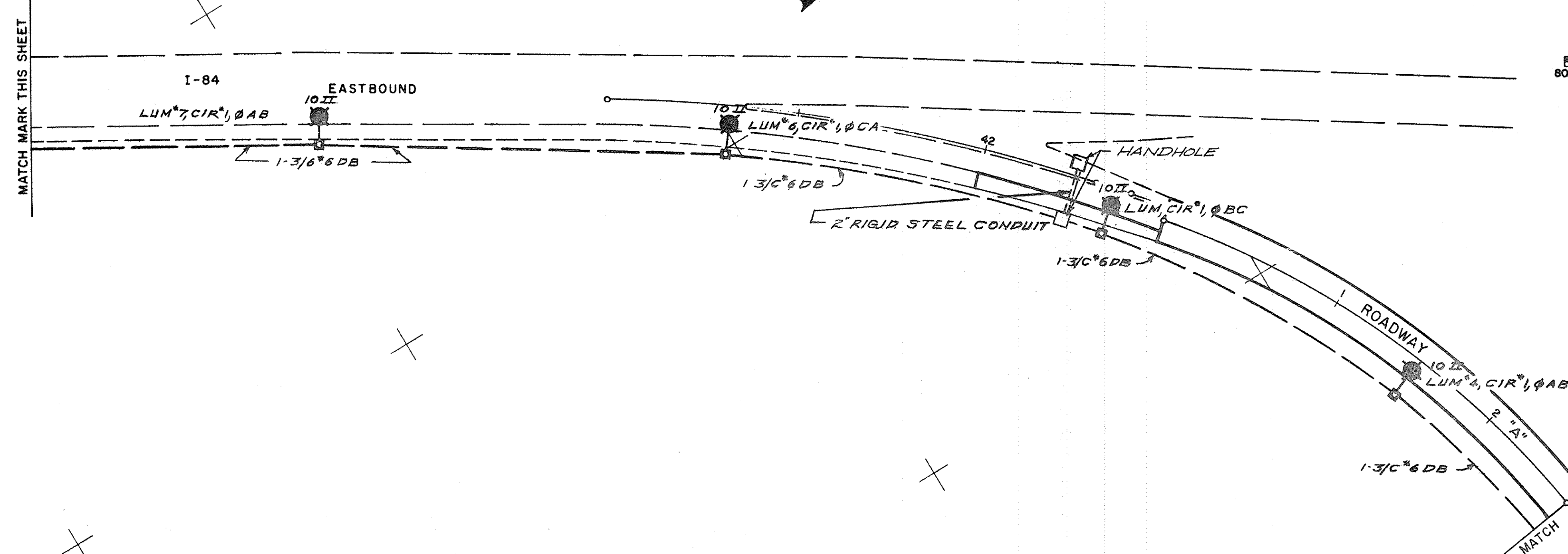
NO.	DATE	REVISION	BY
1	1/1/71	ADDED STORM DRAIN	27B
2	1/1/72		

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	1-84-2(85)41	131-102	1969	1-84	30	78

SAFETY REST AREA

SCALE IN FEET
80 40 0 80

PLAN	NOTE BOOK	NO.	BY	DATE	CHECKED BY	DATE
Surveyed			R. C.	5/18/69	R. C.	5/19/69
Plotted						
Traced						
Designed						
New Const. Noted						
Top. Field Chk. Noted						

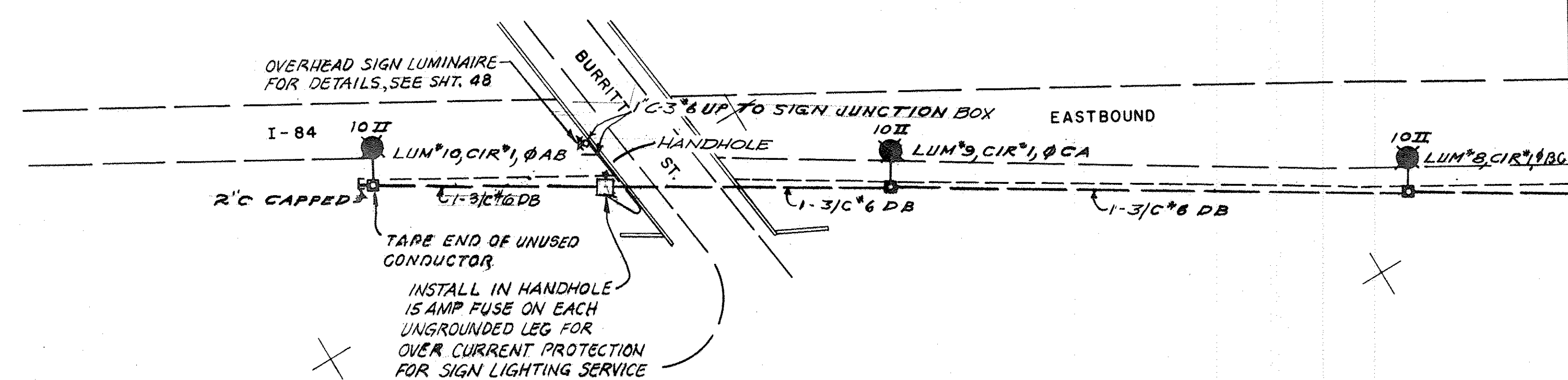


NOTES

1. FOR LEGEND SEE THIS SHEET
2. FOR GENERAL NOTES SEE SHEET NO. 36

LEGEND

- NEW SERVICE POLE
- CONCRETE HANDHOLE
- ⊗ LIGHTING STANDARD
- ⊗ 400 WATT MERCURY VAPOR LUMINAIRE
- ⊗ 250 WATT MERCURY VAPOR LUMINAIRE
- 200 WATT INCANDESCENT WALK LIGHT MOUNTED ON ALUMINUM POLE
- DB ALL REFERENCES TO DB MEAN NONMETALLIC SHEATHED CABLE
- NON METALLIC SHEATHED CABLE, SIZE AS SHOWN ON DRAWING
- RIGID STEEL CONDUIT RUN UNDERGROUND IN TRENCH SIZE AND NO. OF CONDUCTORS AS SHOWN ON DRAWINGS.
- RIGID STEEL CONDUIT SLEEVE UNDER ROAD AND/OR WALKWAY SIZE AS SHOWN ON DRAWINGS.
- 10-II NUMBER-DENOTES LENGTH OF BRACKET FOR LUMINAIRE ROMAN NUMERAL DENOTES TYPE OF A.S.A. DISTRIBUTION
- ⊗ 250 WATT MERCURY VAPOR LUMINAIRE (175 WATT LAMP)



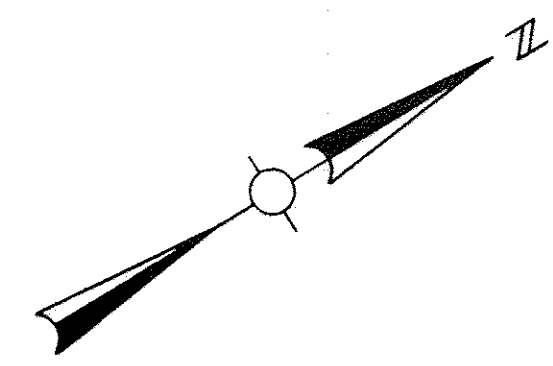
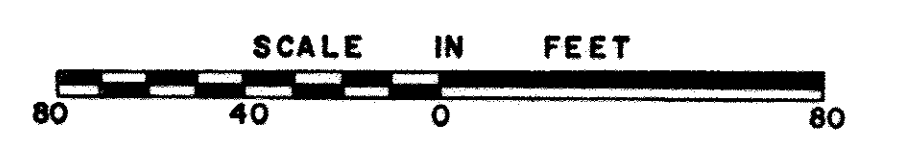
REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

FOR INFORMATION ONLY

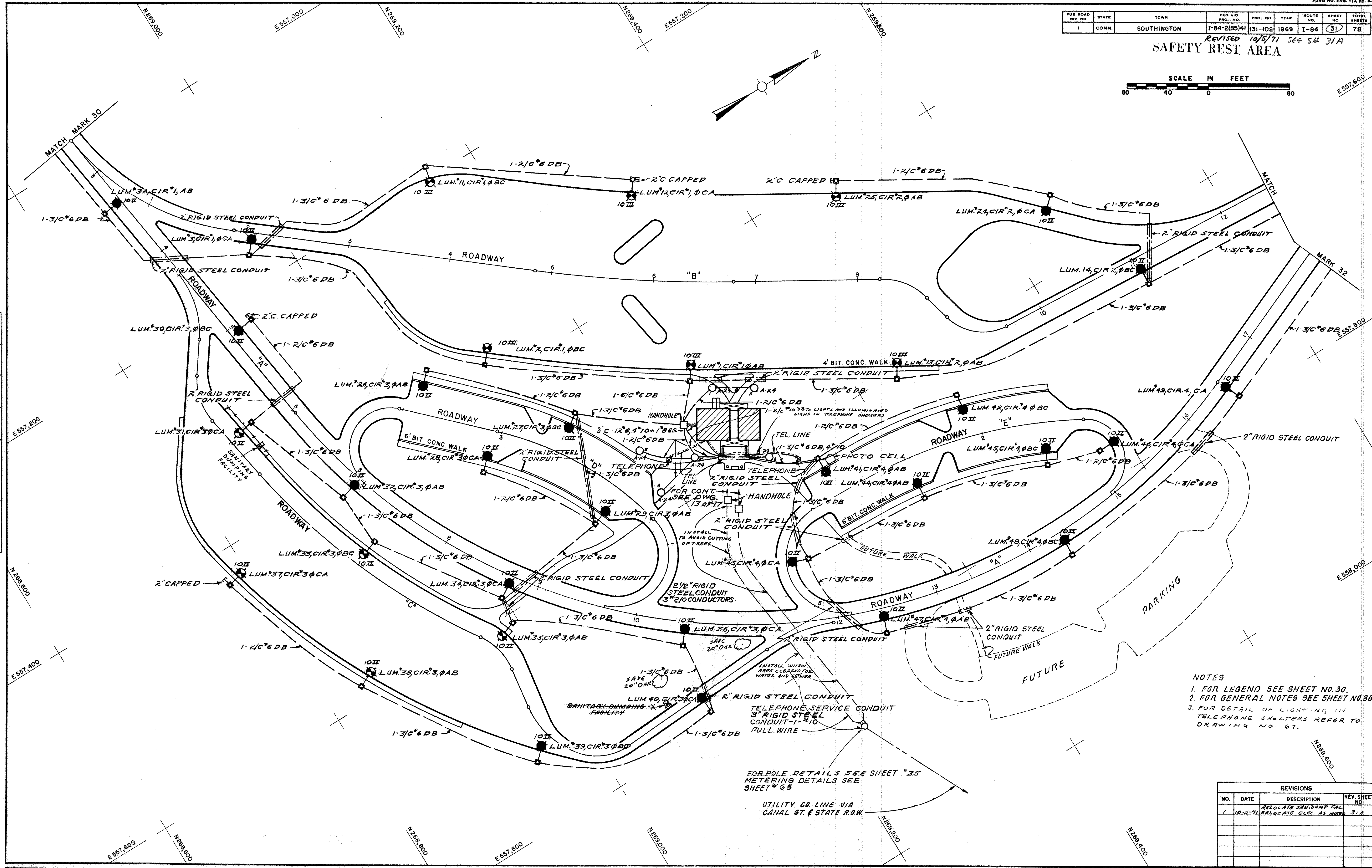
PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	31	78

REVISED 10/5/71 SEE SK 31A

SAFETY REST AREA



PLAN	NO.	DATE	CHECKED BY	DATE	BY	Surveyed	Plotted	Traced	Designed	Drawn	Field Chk. Made



- NOTES
1. FOR LEGEND SEE SHEET NO. 30.
 2. FOR GENERAL NOTES SEE SHEET NO. 36.
 3. FOR DETAIL OF LIGHTING IN TELEPHONE SHELTERS REFER TO DRAWING NO. 67.

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	10-5-71	RELOCATE SAN. DUMP FAC. AS NOTED 31A	31A

FOR INFORMATION ONLY

416-03

ELECTRICAL PLAN

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	32	78

SAFETY REST AREA

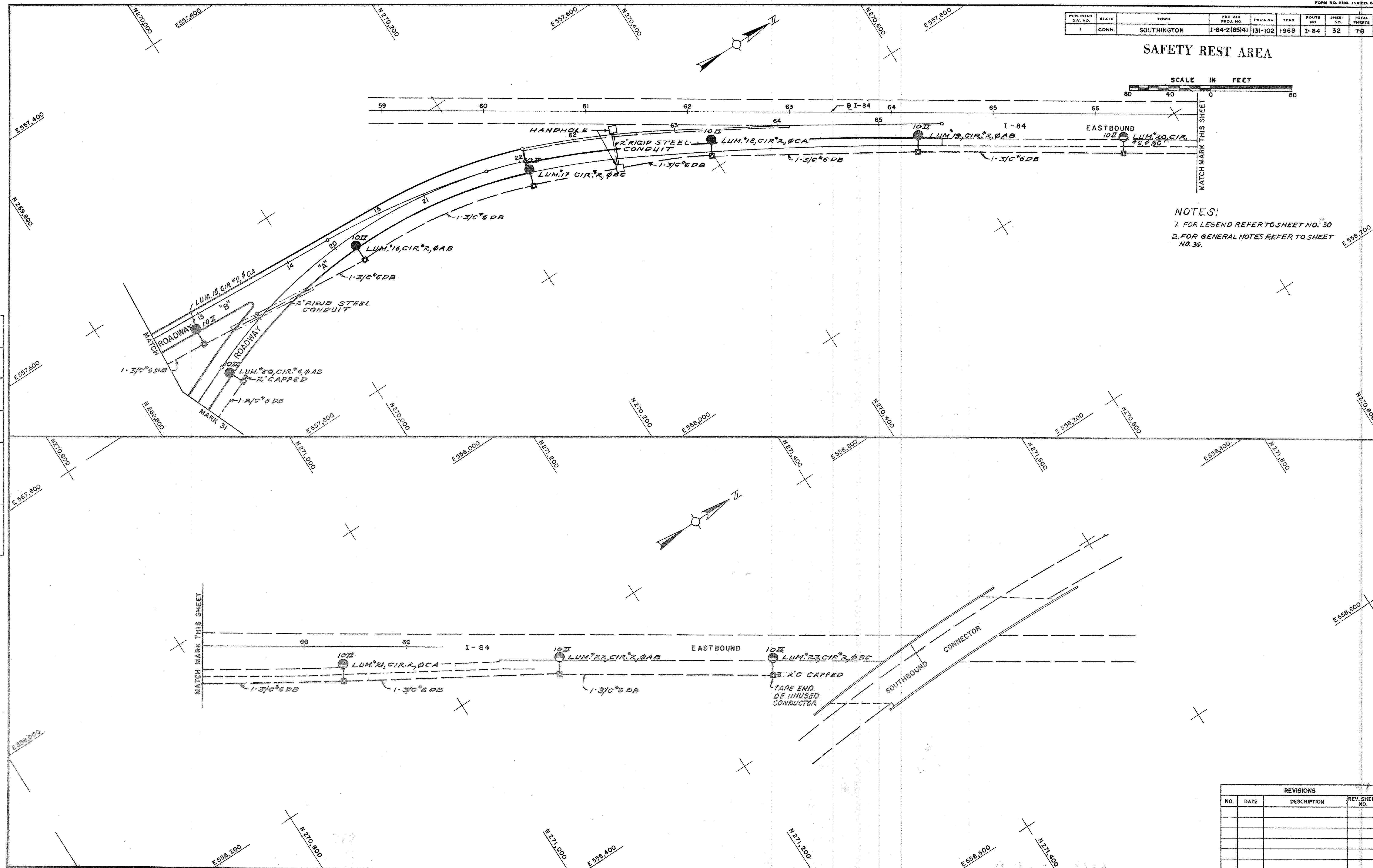
SCALE IN FEET

80 40 0 80

NOTES:

1. FOR LEGEND REFER TO SHEET NO. 30
2. FOR GENERAL NOTES REFER TO SHEET NO. 36.

PLAN	BY	DATE	CHECKED BY	DATE
Supplied Placed	R.C.	5/30/69	A.L.	6/3/69
NOTE BOOK				
NO.				



REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

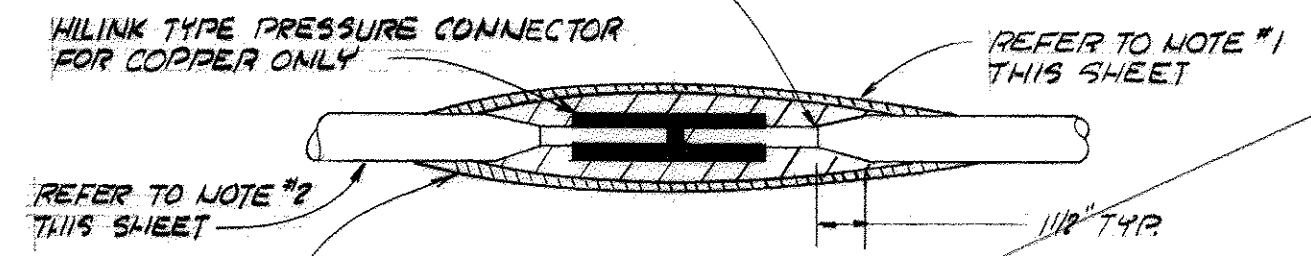
FOR INFORMATION ONLY

416-03

ELECTRICAL PLAN

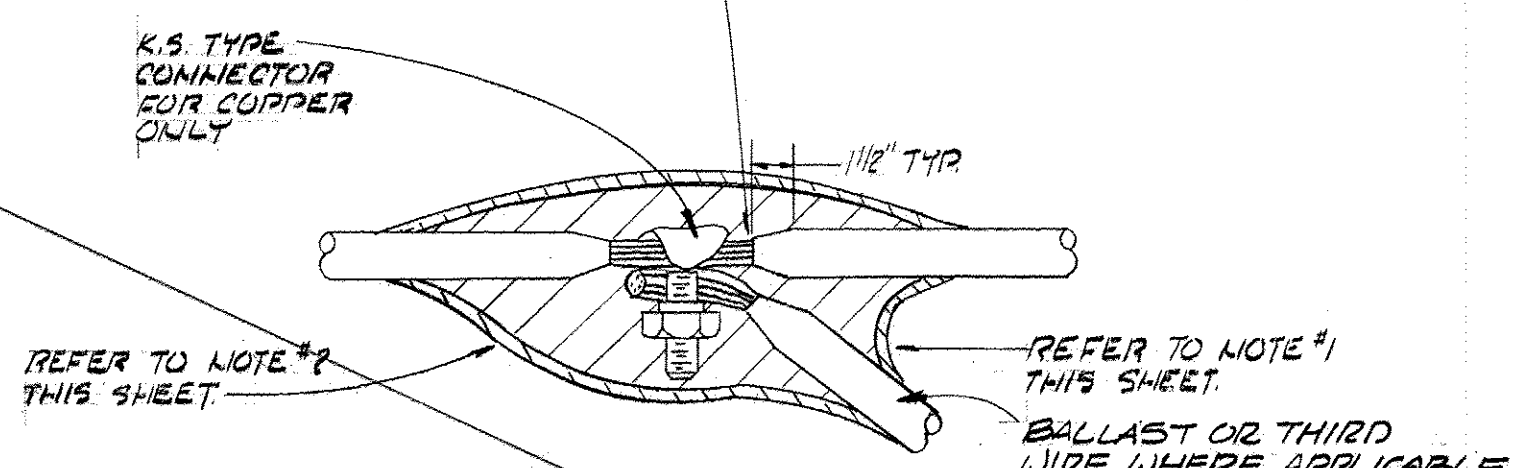
SAFETY REST AREA

APPLY RUBBER SPLICING TAPE WITH APPROXIMATELY 50% OVERLAP TO A THICKNESS OVER THE CONNECTOR 1 1/2 TIMES THE FACTORY APPLIED INSULATION AND TAPE DOWN JACKET AT A POINT APPROXIMATELY 1 1/2" FROM THE EDGE OF PENCIL.



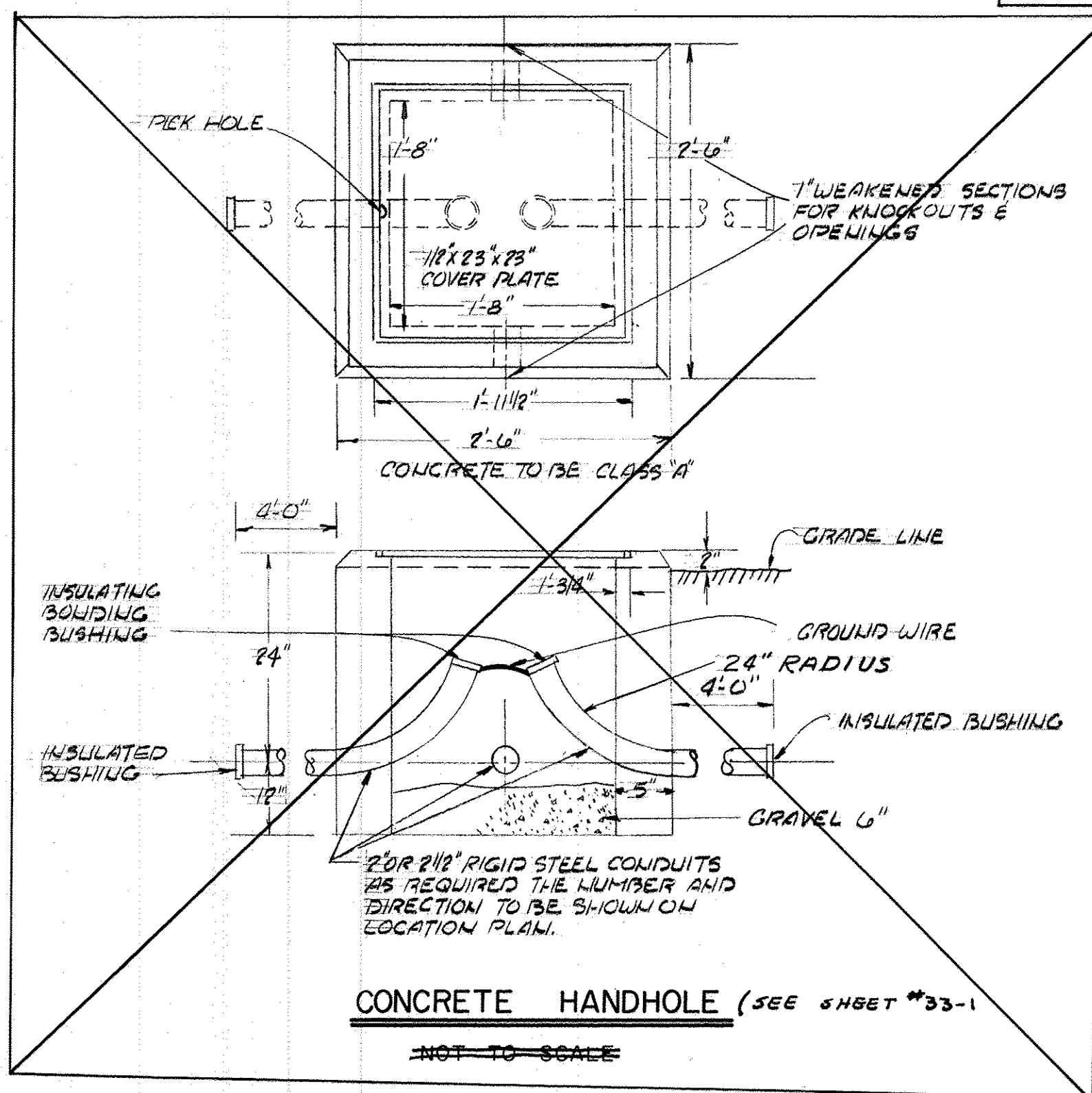
STRAIGHT CONNECTOR
NOT TO SCALE

APPLY RUBBER SPLICING TAPE TO A THICKNESS OVER THE CONNECTOR OF 1 1/2 TIMES THE FACTORY APPLIED INSULATION AND TAPE DOWN JACKET AT A POINT APPROXIMATELY 1 1/2" FROM THE EDGE OF PENCIL.

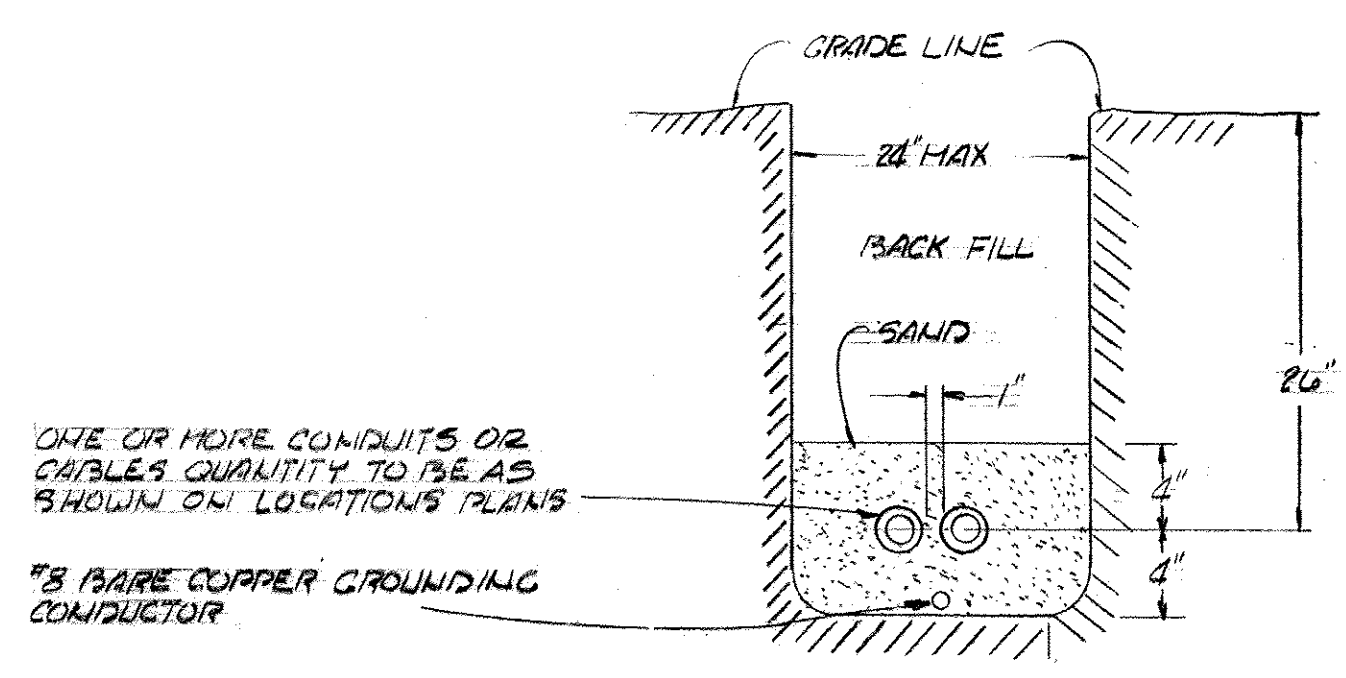


TAP CONNECTOR
NOT TO SCALE

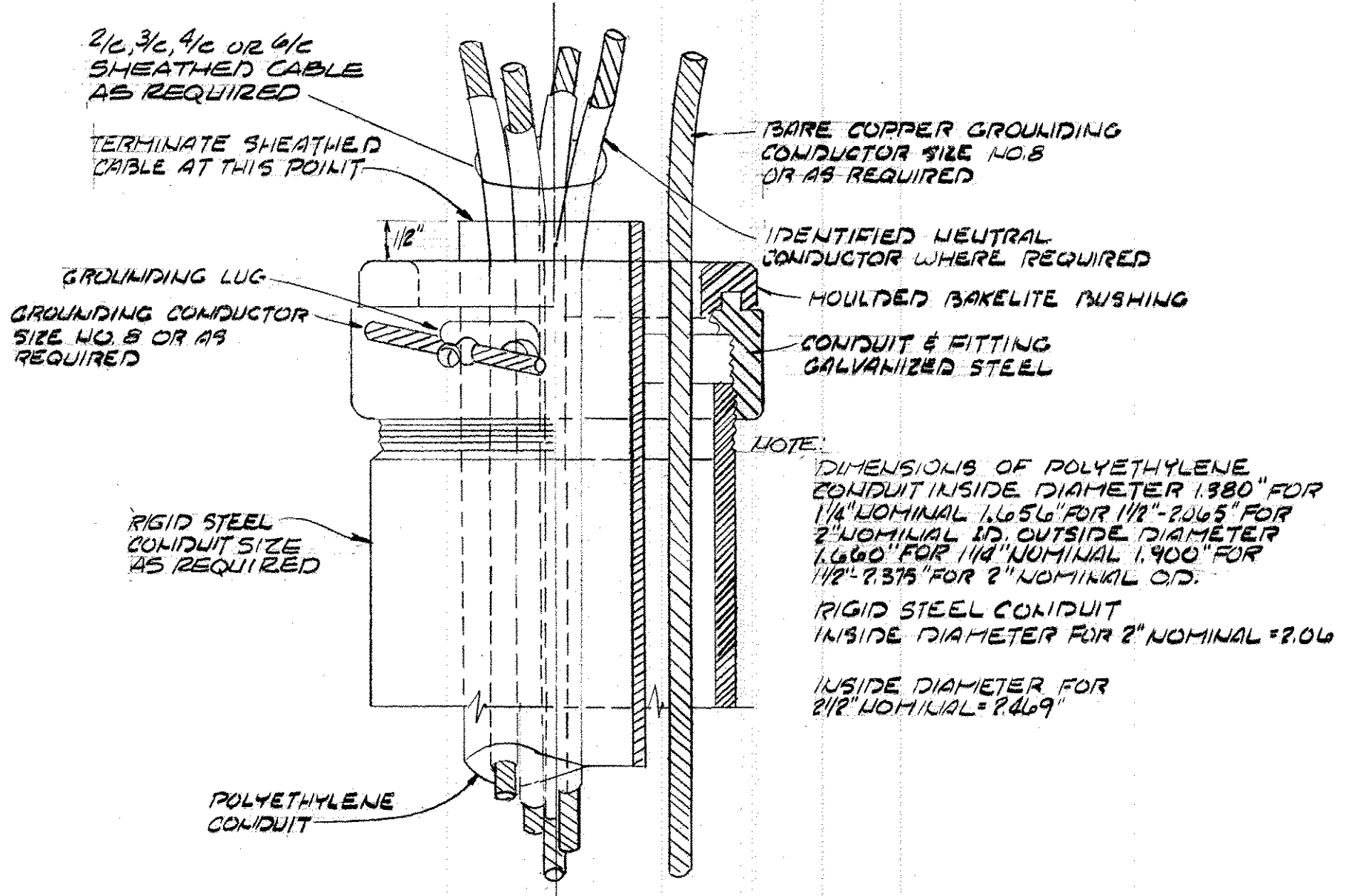
COVER THE ENTIRE SPICE PLUS 1 1/2" OF JACKET AT EACH END WITH 4 LAYERS OF SCOTCH #88 (OR EQUAL) PLASTIC ELECTRICAL TAPE CONSISTING OF TWO TAPES APPLIED SPIRALLY WITH A 50% OVERLAP.



SEE NOTE 5



BURIED CONDUIT
OR SHEATHED CABLE
NOT TO SCALE

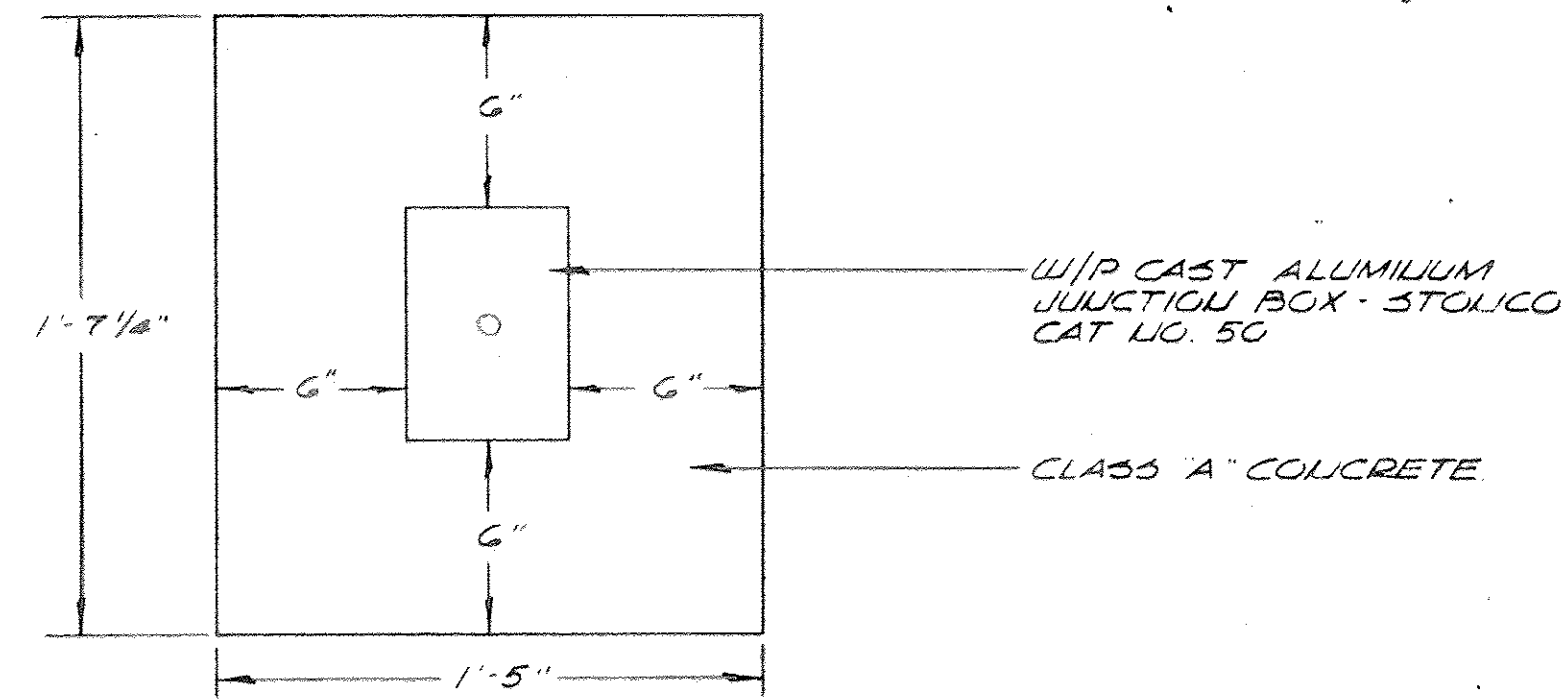


SHEATHED CABLE TERMINATION AT
LIGHT STANDARD, BASE & HANDHOLE
NOT TO SCALE

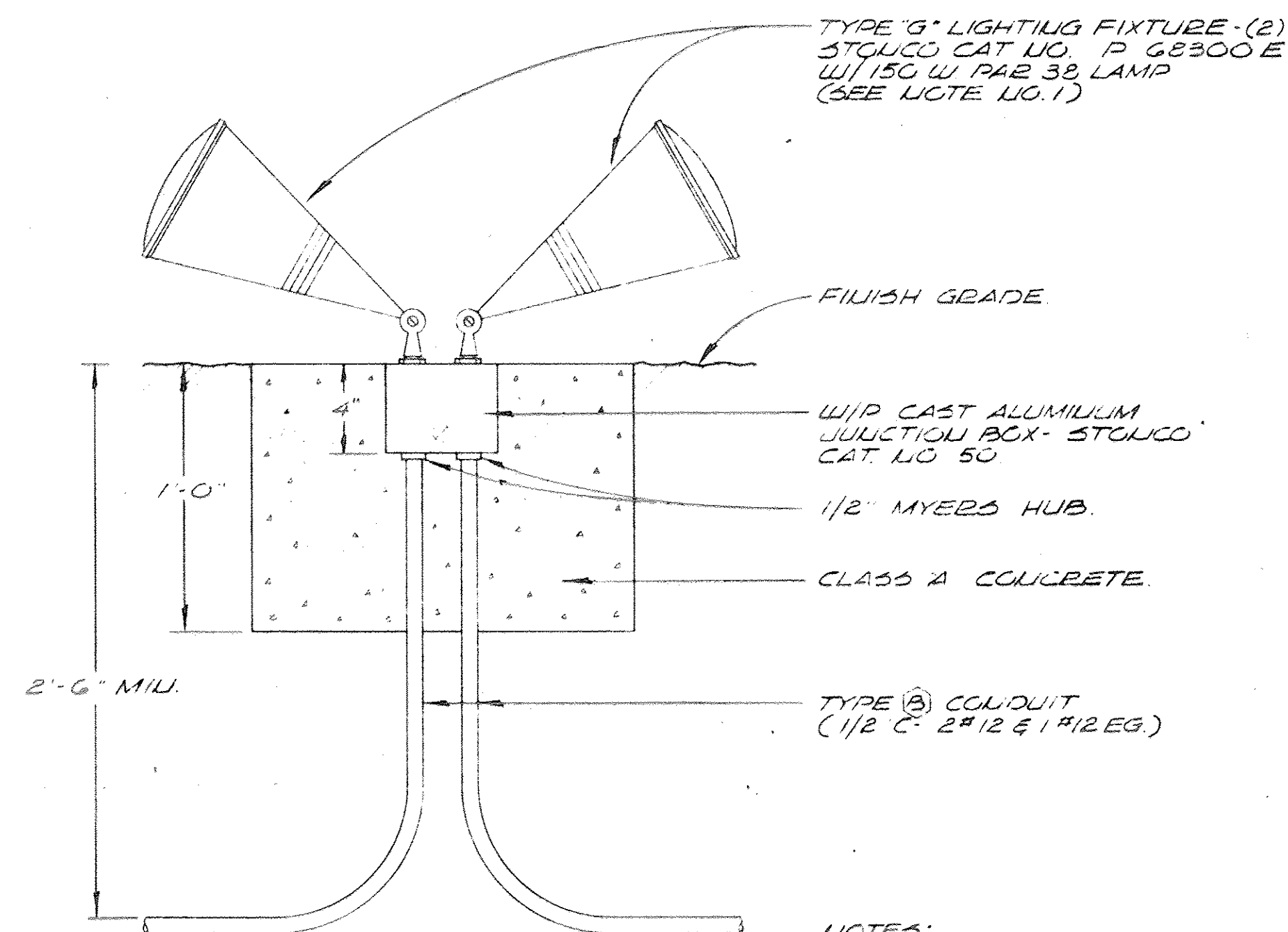
- NOTES
1. THESE SPLICES SHOULD BE AS RECOMMENDED BY THE CABLE MANUFACTURERS BUT IN NO CASES LESS THAN THE CROSS-SECTIONAL DIMENSIONS SHOWN.
 2. PRIOR TO ANOTHER TYPE OF SPICE BEING USED, A TYPICAL CROSS-SECTION SHALL BE SUBMITTED TO THE ENGINEER FOR HIS APPROVAL.
 3. FOR GENERAL NOTES REFER TO SHEET NO. 36
 4. FOR LEGEND REFER TO SHEET NO. 30
 5. SPLICES WHERE REQUIRED SHALL BE MADE BY THE USE OF PRE-MOLDED, WATERPROOF CONNECTOR KITS OF THE TYPE APPROVED BY THE ENGINEER

REVISIONS		
NO.	DATE	DESCRIPTION
1	10/5/71	ADD NOTE 6 REVISE CONC. HANDHOLE

S.P.R. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON	I-84-2(85)41	131-102	1969	I-84	33 B	78

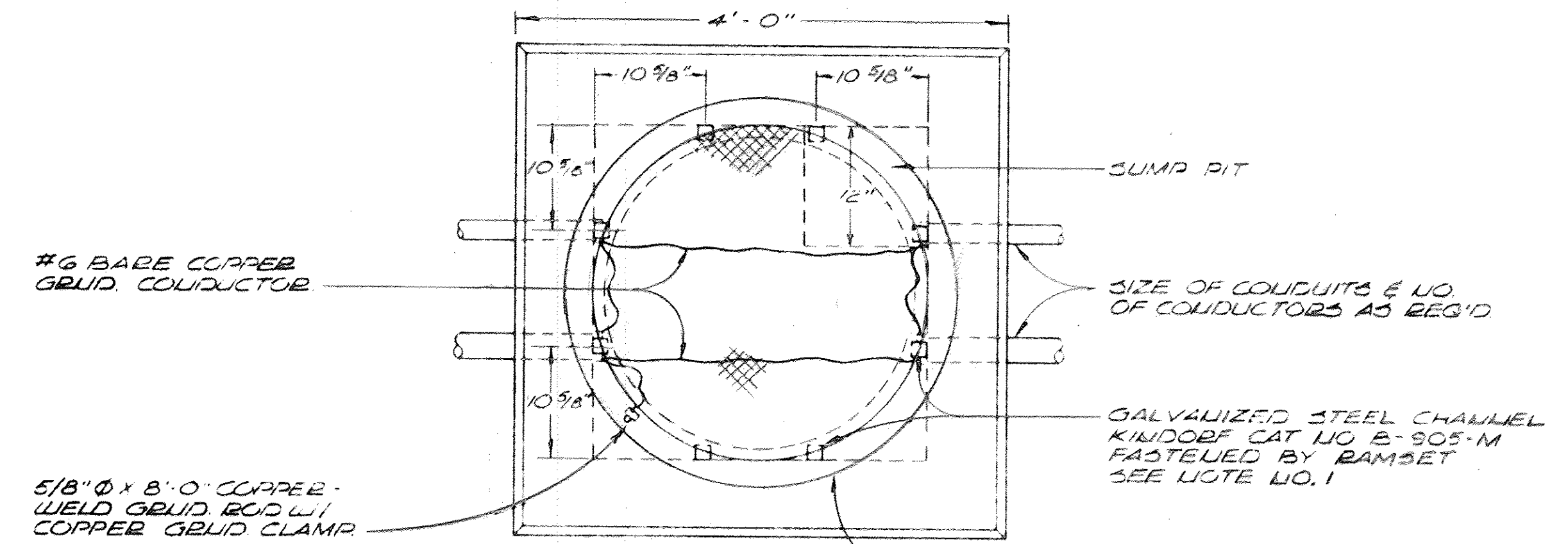


PLAN OF CONCRETE

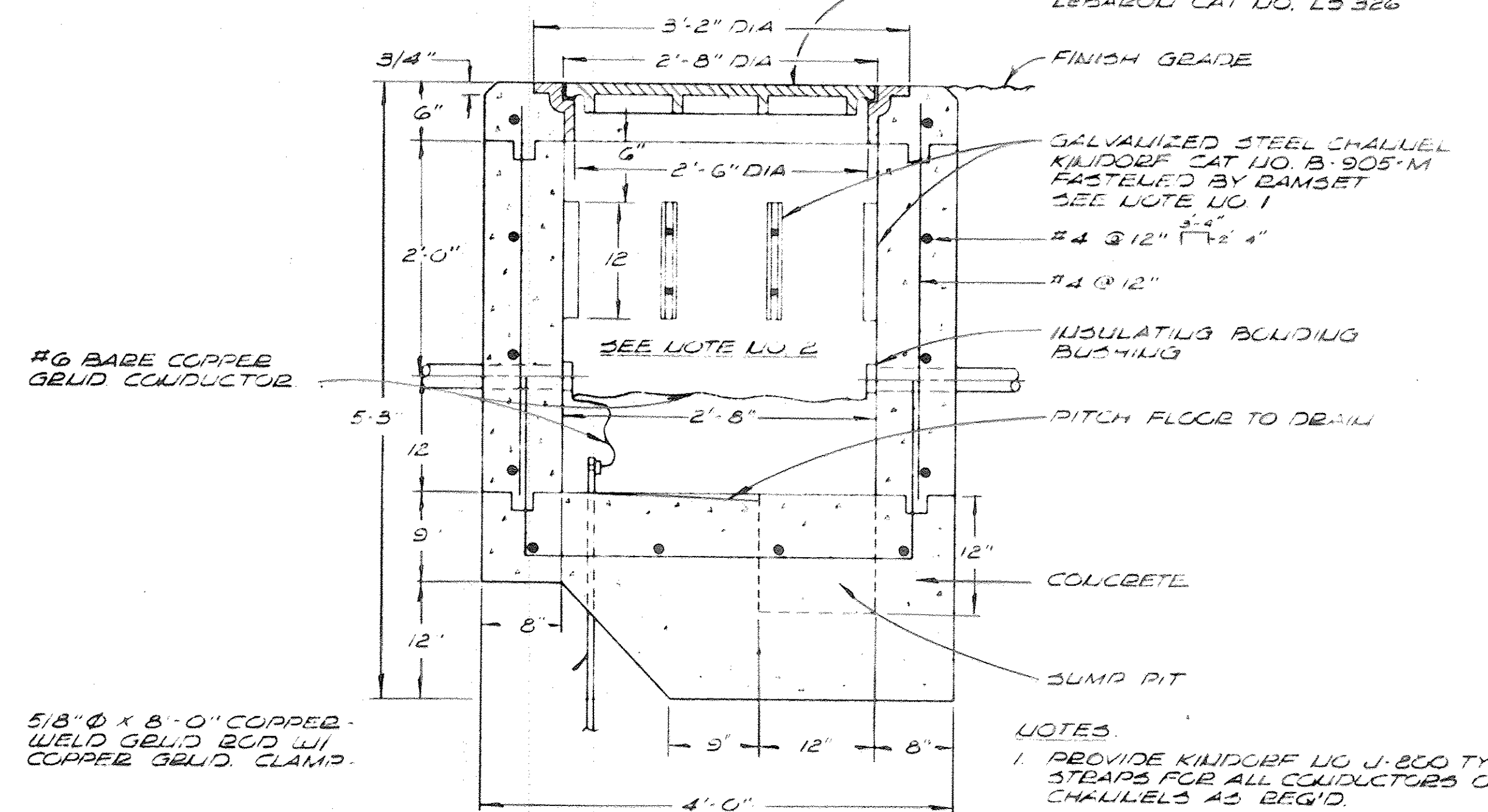


TYPE "G" LIGHTING FIXTURE
NOT TO SCALE

- NOTES:
- TYPE "F" FIXTURE - SAME DETAIL W/ (1) STUCCO, P68300E IN CENTER K.O.



PLAN



SECTION

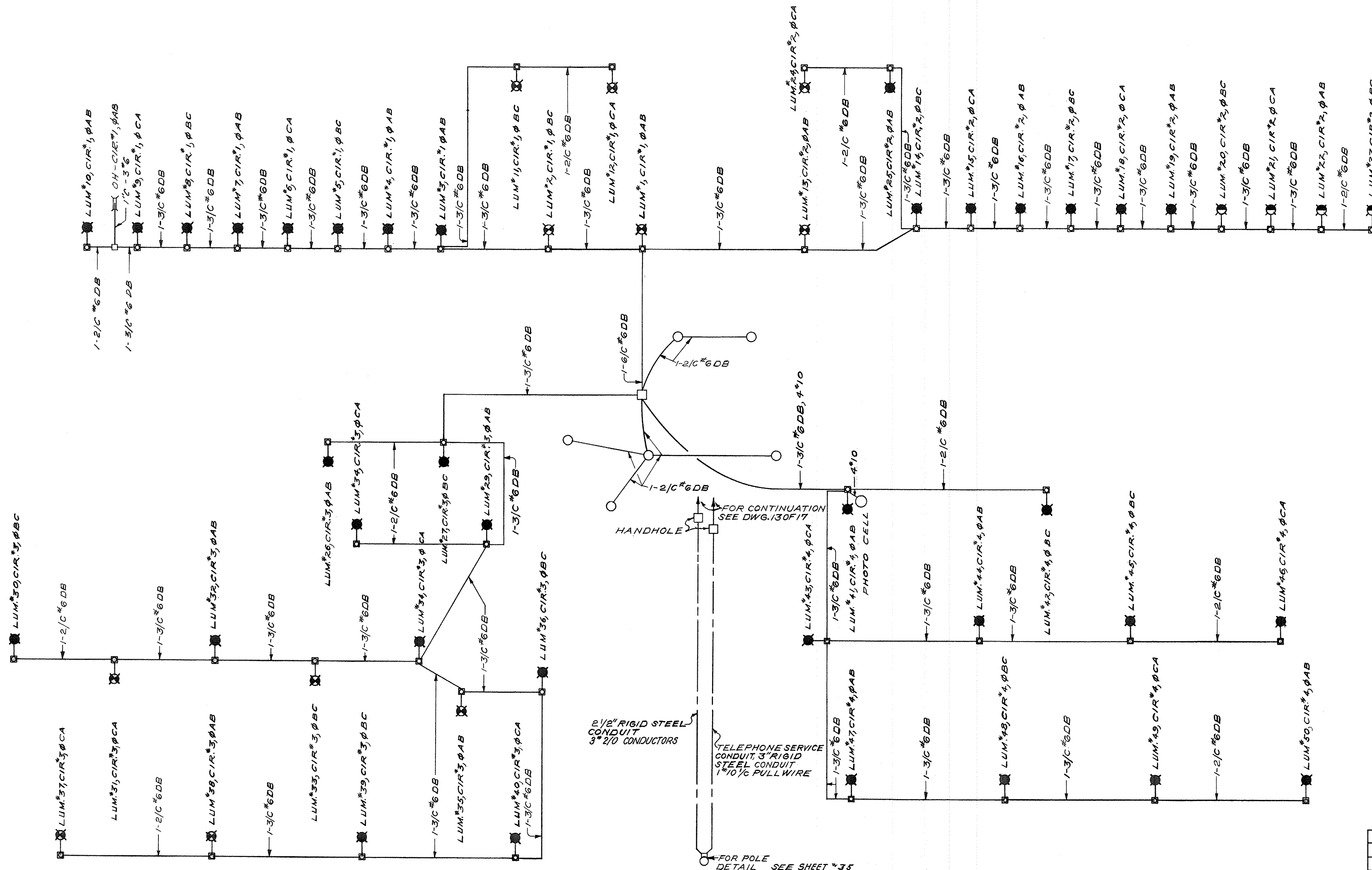
CONCRETE MANHOLE
NOT TO SCALE

- NOTES:
- PROVIDE KINDORE NO. J-800 TYPE STRAPS FOR ALL CONDUCTORS ON CHANNELS AS REQ'D.
 - ALL CONDUCTORS ENTERING HAND-HOLES (ALL TYPES) SHALL BE BACKED UP WITH KINDORE CHANNELS WITH STRAPS AS INDICATED. EACH CONDUCTOR SHALL BE TAGGED WITH FIBERPROOF TAG AS NOTED IN SPECIFICATIONS & SHALL BE LONG ENOUGH TO GO OVER AROUND PERIMETER OF MANHOLE & 12" FOR SPICE & COLLECTOR.

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	10/15/71	TYPE "G" LIGHTING FIXTURE REVISED CONCRETE MANHOLE	33 B

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
CONN		SOUTHINGTON	I-84-2(85)41	131-102	1969	1-84	34	76

SAFETY REST AREA



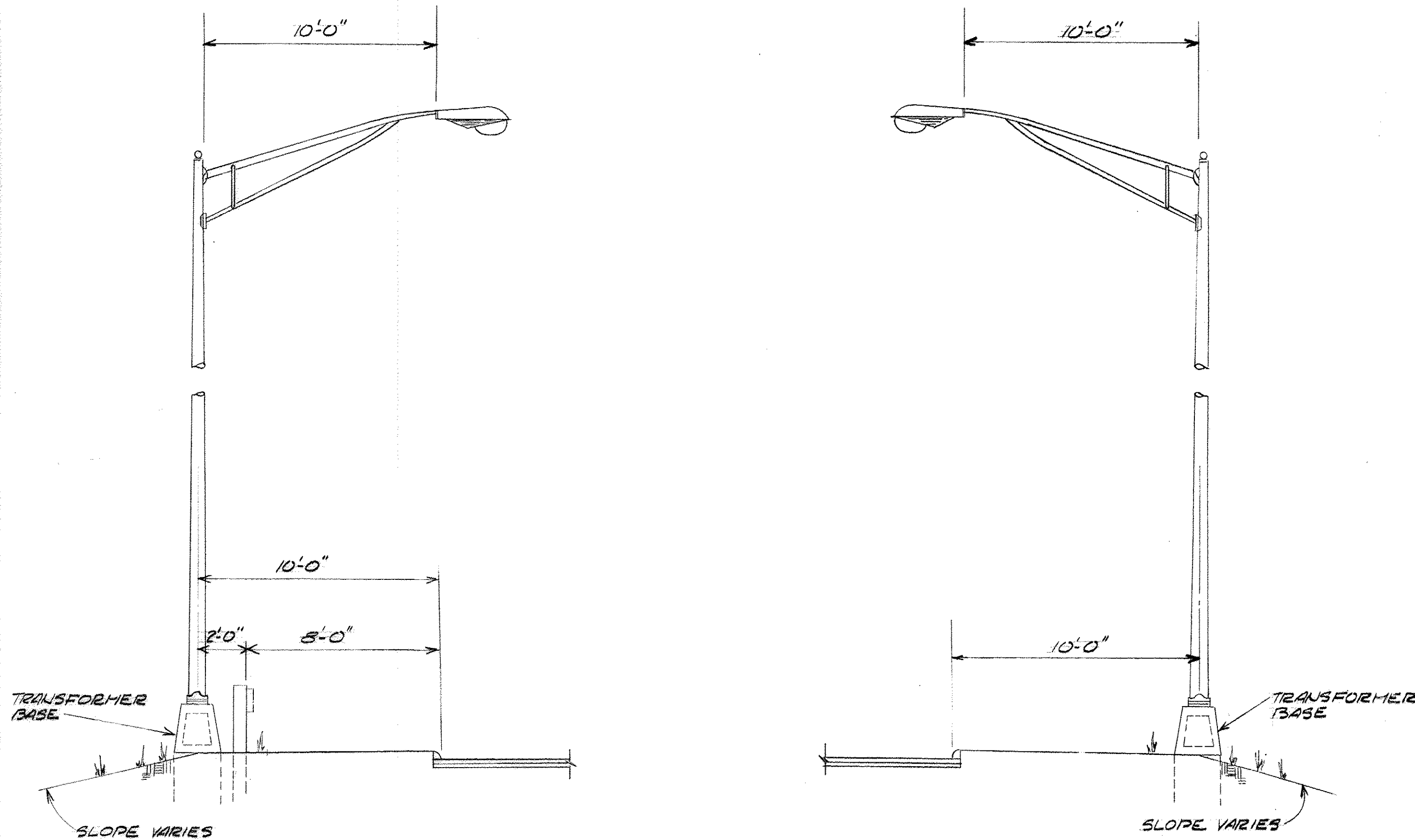
PLAN	BY	DATE	CHECKED BY	DATE
Surveyed				
Plotted				
Traced	R.A.	5/12/69	R.C.	5/13/69
NOTE BOOK	NO.	NO.	NO.	NO.
Designated				
New Const. Noted				
Top. Field Chk. Noted				

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

SAFETY REST AREA

GENERAL NOTES

1. LIGHT SOURCE - MERCURY VAPOR
2. LAMP SIZES ARE 400, 250 & 175 WATT A.S.A.
LAMP DESIGNATION H33-100, H37-5K13 &
H39-22K13 RESPECTIVELY
3. TYPE OF DISTRIBUTION IS INDICATED BY ROMAN
NUMERAL ADJACENT TO THE LUMINAIRE
SYMBOL ON PLAN SHEETS
4. NUMBER SHOWN OVER LUMINAIRE SYMBOL
ON PLAN SHEETS INDICATES LENGTH OF
BRACKET ARM
5. LIGHT STANDARD CLEARANCES ARE MEASURED FROM
EDGE OF PAVEMENT OR FACE OF CURB - SEE
PLAN SHEETS FOR OFF SETS.
6. MOUNTING HEIGHT IS 30'-0"
7. SPACING OF LUMINAIRES AS INDICATED ON PLANS.
8. FOR ROADWAYS AND PARKING AREAS:
a. INITIAL LEVEL OF ILLUMINATION IS 1.0 FT.-C.
b. AVERAGE MAINTAINED ILLUMINATION IS .6 FT.-C.
c. RATIO OF UNIFORMITY IS BETWEEN 3:1 TO 4:1
d. DESIGN MAINTENANCE FACTOR IS .6
9. MULTIPLE CIRCUITS - COILABLE POLYETHYLENE CONDUIT
WITH PRE-ASSEMBLED CONDUCTORS SIZE AS INDICATED
ON PLANS.
10. INSTALL (1) SEPARATE #8 AWG. BARE COPPER GROUNDING
CONDUCTOR WITH ALL ROADWAY LIGHTING CONDUCTORS.
11. THE FINISH ELEVATION OF THE TOP & CENTER OF
THE CONCRETE BASE SHALL BE APPROXIMATELY
LEVEL WITH THE EXISTING GROUND.
12. PROVIDE 6-200 WATT INCANDESCENT POLE MTD
WALK LIGHTS TO MATCH PORCH LIGHTS ON REST ROOM
BUILDING. POLE TO 10'-0" ALUMINUM MINIMUM -
CONNECT TO CIRCUIT NO. 24 IN PANEL 24 INSIDE
REST ROOM BUILDING THROUGH A 2-POLE 40 AMP
250 V. TIME CLOCK TO BE MOUNTED ON SERVICE
PANELBOARDED IN BASEMENT. REFER TO DWG.
NO. 12 OF 17 FOR DETAILS.



NORMAL SECTIONS-TRUCK, CAR AND/OR
PARKING ENTRANCE ROADWAYS

NOT TO SCALE

REVISIONS		
NO.	DATE	DESCRIPTION

CONNECTICUT DEPARTMENT OF TRANSPORTATION

PLAN FOR CONSTRUCTION OF INTERSTATE ROUTE 691

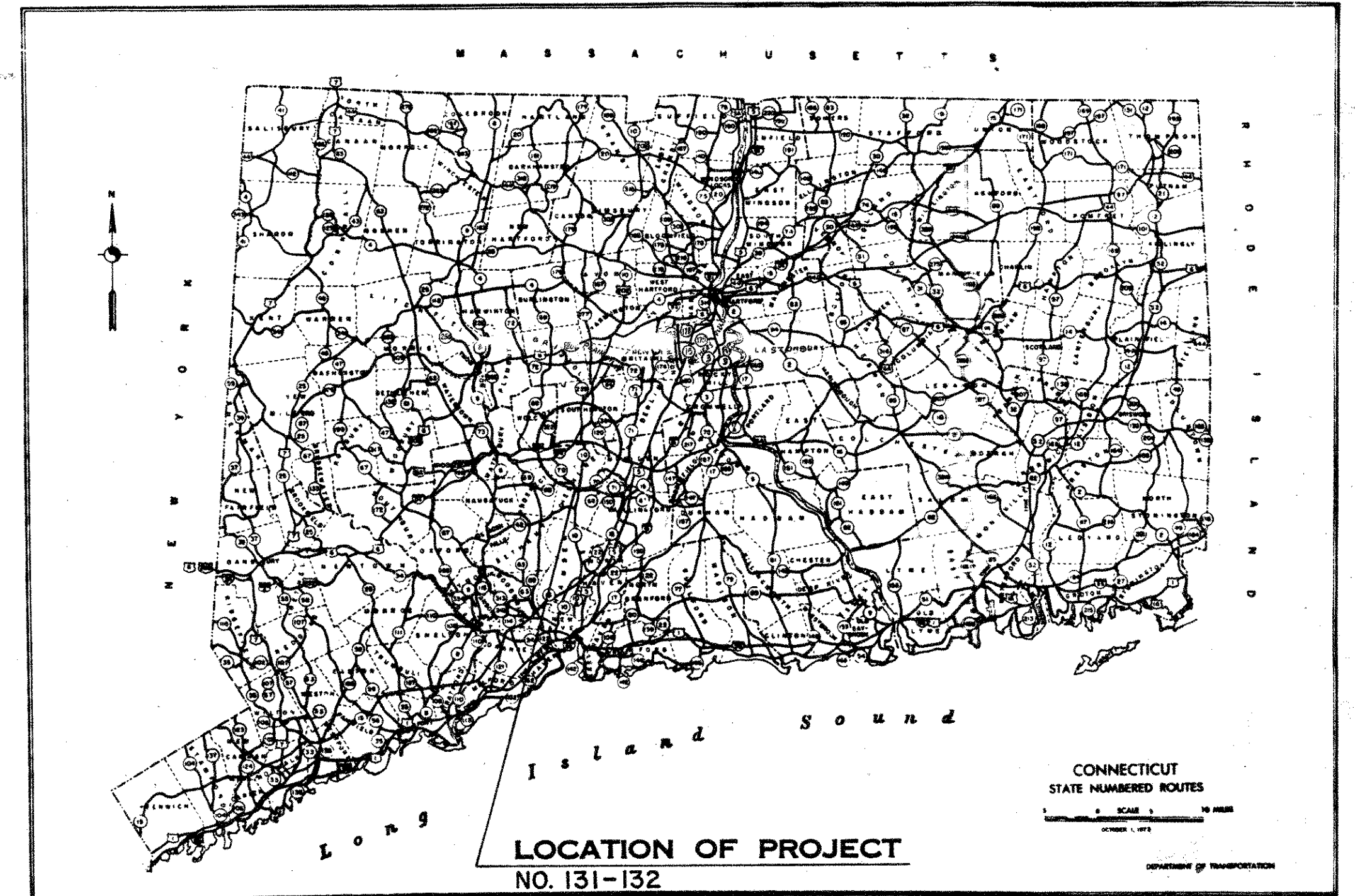
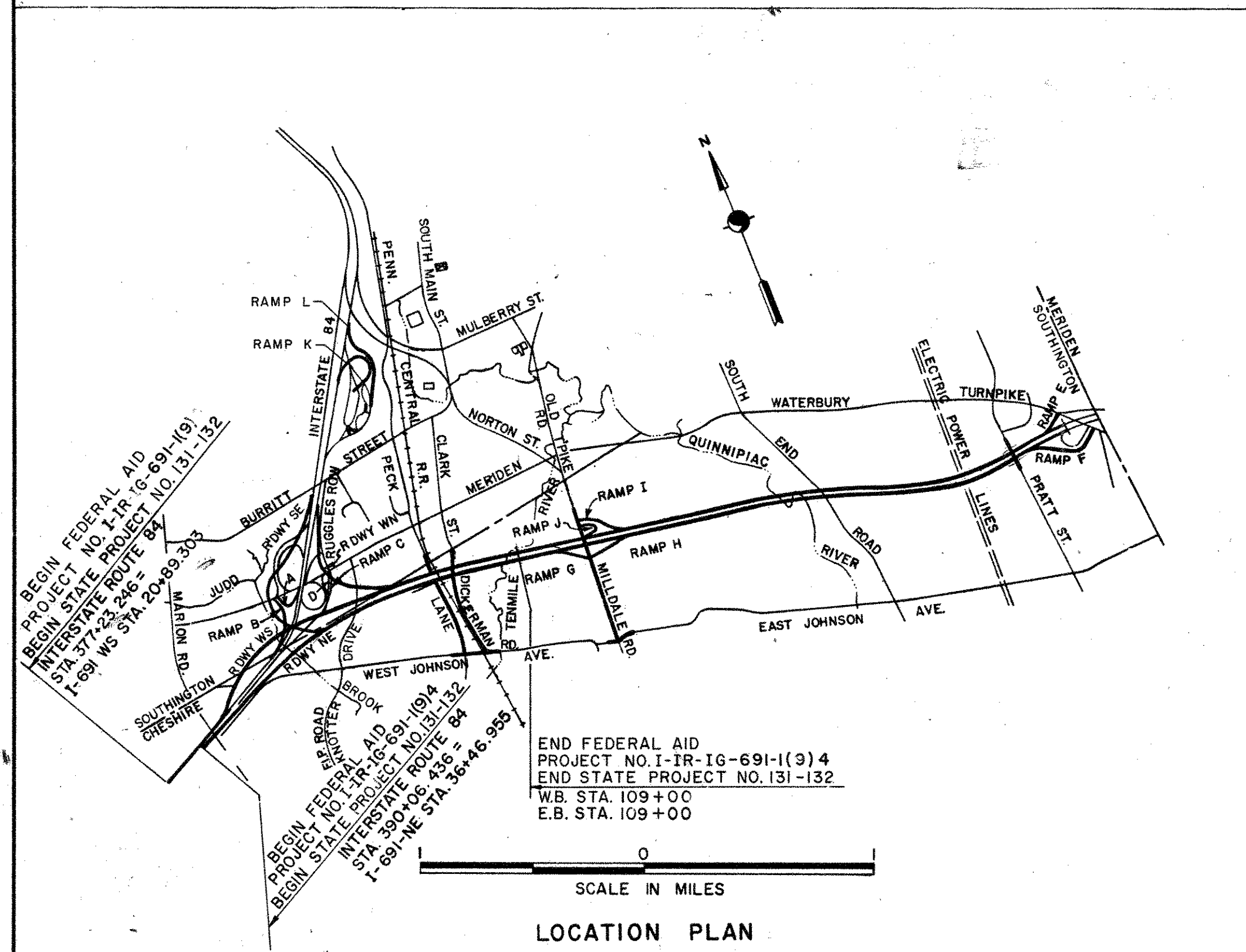
IN THE TOWNS OF
CHESHIRE AND SOUTHTON

FROM STA. I-691 W.S. 20+89.303 TO STA. I-691 W.B. 109+00
I-691 N.E. 36+46.955 I-691 E.B. 109+00

LENGTH 8031.871 FT.

PLAN 1 IN. = 40 FT.
SCALES PROFILE HOR. 1 IN. = 40 FT. VERT. 1 IN. = 4 FT.
CROSS SECTIONS 1 IN. = 10 & 5 FT.
OTHER SCALES AS NOTED
TO BE MAINTAINED BY THE STATE OF CONNECTICUT

FEDERAL AID PROJECT NO. I-IR-IG-691-I(9)4



1980 SPECIFICATIONS, FORM NO. 812 INCLUDING
SUPPLEMENT THERETO DATED JAN. 1, 1984 GOVERN
ALL ELEVATIONS ON THIS PROJECT
BASED ON NATIONAL GEODETIC VERTICAL DATUM OF 1929.

TOWN NO. 131
PROJECT NO. 132

Construction Started 9/5/1984
Construction Completed
Plans Revised by J. KRUCKER
Plans Checked by

ASSUMED DESIGN SPEED = 70 MPH
ANTICIPATED D.D.H.V. IN 2000
INTERSTATE 691 2100 VEHICLES PER HOUR
ANTICIPATED A.D.T. IN 2000
INTERSTATE 691 31,200 VEHICLES PER DAY
CONN. D.O.T. CLASSIFICATION = F. URBAN RESIDENTIAL
CONTROL OF ACCESS = LIMITED ACCESS
1976 GEOMETRIC HIGHWAY DESIGN STANDARDS.
(REVISED TO DATE)

SHEET NO. 188-I HAS BEEN ADDED TO THESE PLANS
TOTAL NUMBER OF SHEETS IS ACTUALLY 778.

WHEREVER ON THE PLANS THE "FEDERAL AID PROJECT
NO. I-691-I(9)4" APPEARS, IT SHALL BE CONSTRUED TO
READ "FEDERAL AID PROJECT NOS. I-IR-IG-691-I(9)4".

These plans have been prepared for the State of Connecticut
Department of Transportation Bureau of Highways by
P.R.C. Harris, Consulting Engineers, except those sheets
identified as utility plan sheets and appended to these plans
have been prepared by the utility owners. The undersigned
hereby disclaims all responsibility for the accuracy of data shown
on the above mentioned utility sheets, except that he has reviewed
them for general coordination with the plans prepared by him.

DESIGNED BY:
P.R.C. HARRIS

PER: Frank E. Lo Presti
SENIOR VICE PRESIDENT
FRANK E. LO PRESTI
CONN. PROF. ENG. REG. NO. 9306
DATE Sept. 1983

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:

DIVISION ADMINISTRATOR

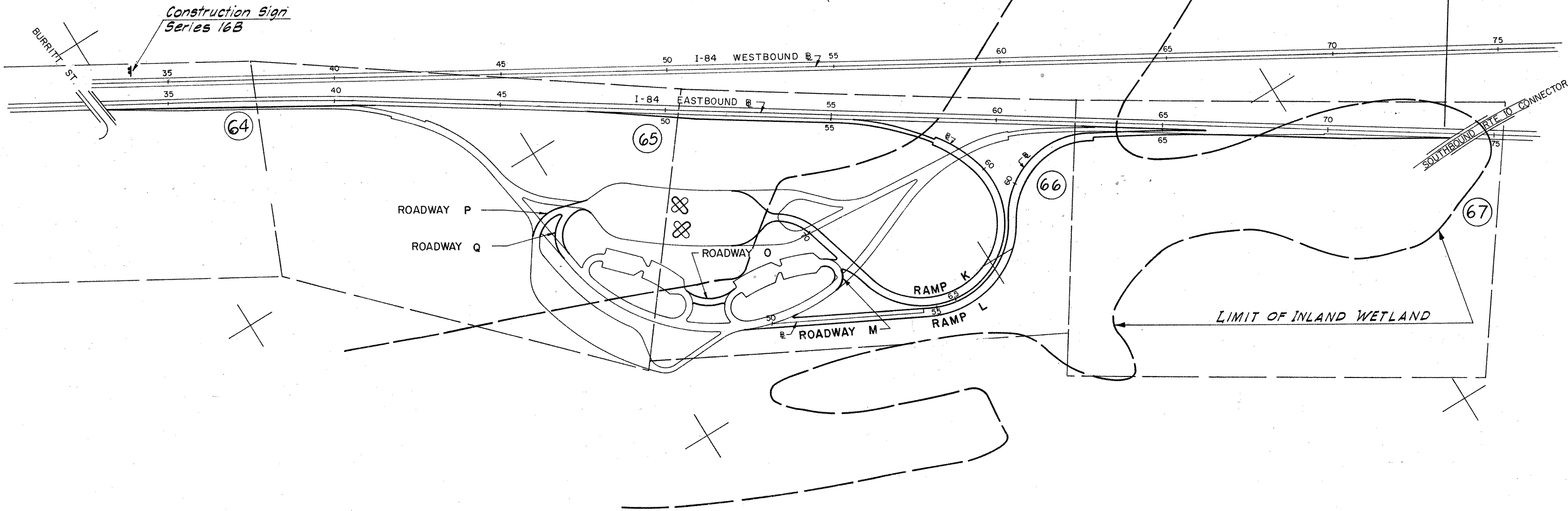
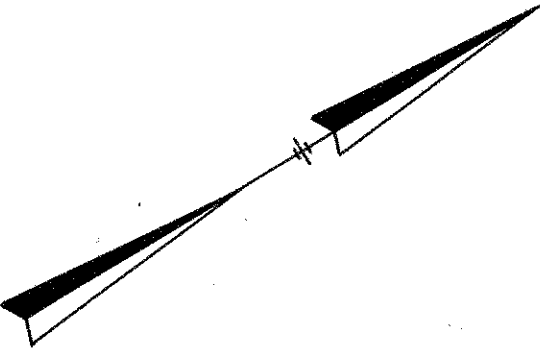
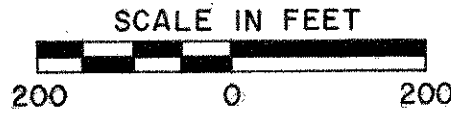
DATE

LIST OF DRAWINGS

LIST OF DRAWINGS				FHWA APPROVAL DATE
SHEET NO.	TITLE	SHEET NO.	STANDARD SHEETS	
1	Title Sheet	201	Disposal of Unsuitable Material	9-30-69
2-6	Index Plans	220-F	Metal Beam Rail (Type R-I)	6-22-73
7	Federal Aid Break Down Sheet	220-G	Metal Beam Rail (Type MD-I)	6-22-73
8-16	Detailed Estimate Sheets			
17-20	Typical Cross Sections	221-F	Endwalls, Slope Paved Inlets & Outlets	3-11-70
21-37	Miscellaneous Details	221-G	Type "D", "D-G" & "L" Endwalls	3-11-70
38-40	Topsoil Plans	222-C	Curbing	11-16-66
41-48	Grading Plans for Roadway Intersections	222-D	Sidewalks and Drives	3-22-72
43	Grading and Striping Plans for Rest Area and Commuter Lot	226-A	A.C.C.M. Pipe Installation in Earth and Rock Slope	10-13-65
44	Curve Data Tables	228-C	Type "C-L" Catch Basin and Drop Inlet	2-18-76
45-49	Detour A and B at Interstate Route 84, Sideroad Detours	228-D	Steel Frame & Grate-Type "A" for Type "C", "C-L" & "C-G" Catch Basin & Drop Inlet	2-25-76
50-54	Consolidator Slope Plans	228-E	Type "C" Catch Basin and Drop Inlet	2-18-76
55-75	Plans	228-F	Steel Frame & Grate-Type-B for Type "C", "C-L" & "C-G" Catch Basin & Drop Inlet	3-27-67
76-81	Profiles - Turning Roadway NE and Eastbound			
82-91	Profiles - Turning Roadway WS and Westbound	507-A	Manhole, Frame and Cover	8-30-77
92-95	Profiles - Turning Roadway SE	507-D	Type "C" Catch Basin Double Grate Type I and II	7-30-79
96-99	Profiles - Turning Roadway WN	507-E	Type "C-L" Catch Basin Double Grate Type I and II	7-30-79
100-101	Profiles - Interstate Route 84 Widening	507-H	Precast Concrete Catch Basin Type "C" or "C-L"	3-16-79
102-107	Profiles - Ramp A thru Ramp D	652-A	Metal and R.C. Culvert End	12-29-78
108-111	Profiles - Ramps E and F	711-A	Figures for Dates on Bridge Parapets	7-28-66
112-115	Profiles - Rock Lane	712-C	Barricades, Service Bridges and Slope Paving	8-16-79
116-117	Profiles - B & M Track Relocation	751-A	Underdrains, Outlets, Perforated Pipe and Slotted R.C. Pipe	8-30-77
118-121	Profiles - Dickerman Road and West Johnson Avenue			
122-125	Signage Plans	882-A	Precast Concrete Median Barrier Curb for Temporary Traffic Control	4-2-79
126-129	T-91/Rte 66 Signing Plans			
130-135	Illumination Plans	913-A	Chain Link Fence	8-01-83
136-137	Signal Plans	918-A	Three Cable Guide Railing (I Beam Posts)	7-28-80
138-176	Bridge Drawings	918-B	Three Cable Guide Railing (I Beam Posts)	7-28-80
177-177	Cross Sections	925-A	Pavement for Railing	4-22-76
A1, A2, B1	Connecticut Light & Power Electric Transmission & Gas Lines			
C1 - C4	American Telephone & Telegraph			
D1 - D3	Town of Cheshire - Sanitary Sewer			
E1	Algonguin Gas Co.			
F1 - F3	Connecticut Light And Power Co.			
G1 - G2	South Central Connecticut Regional Water Authority			
H1 - H8	Southern New England Telephone Co.			

F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Southington-Cheshire	I-691-1(9)4	131-192	1983	I-691	4	777

CONSTRUCTION OF
INTERSTATE ROUTE 691



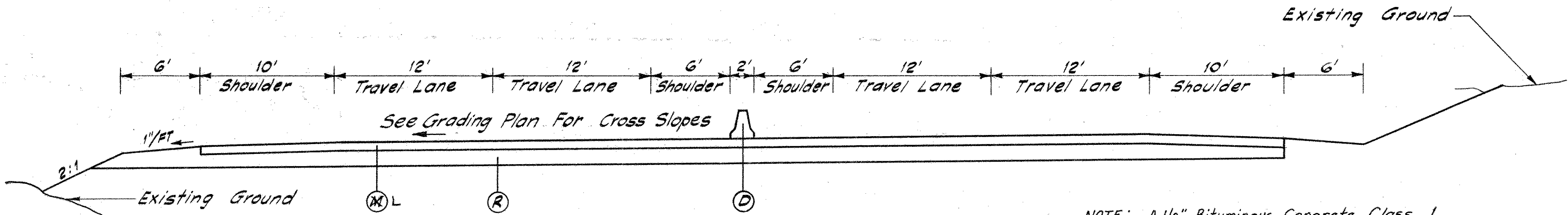
PLAN	NO.	NOTED	DATE	BY	CHECKED BY	DATE
Surveyed						
Plotted						
Traced						
Designed						
Noted						
Top. Field Chk. Noted						

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

TYPICAL CROSS SECTIONS
NOT TO SCALE

THIS SHEET CORRECTED										FORM NO. ENG. 11 ED. 4-71	
F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS			
1	CONN.	Southington-Cheshire	I-691-1(9)4	131-132	1983	I-691	20	777			

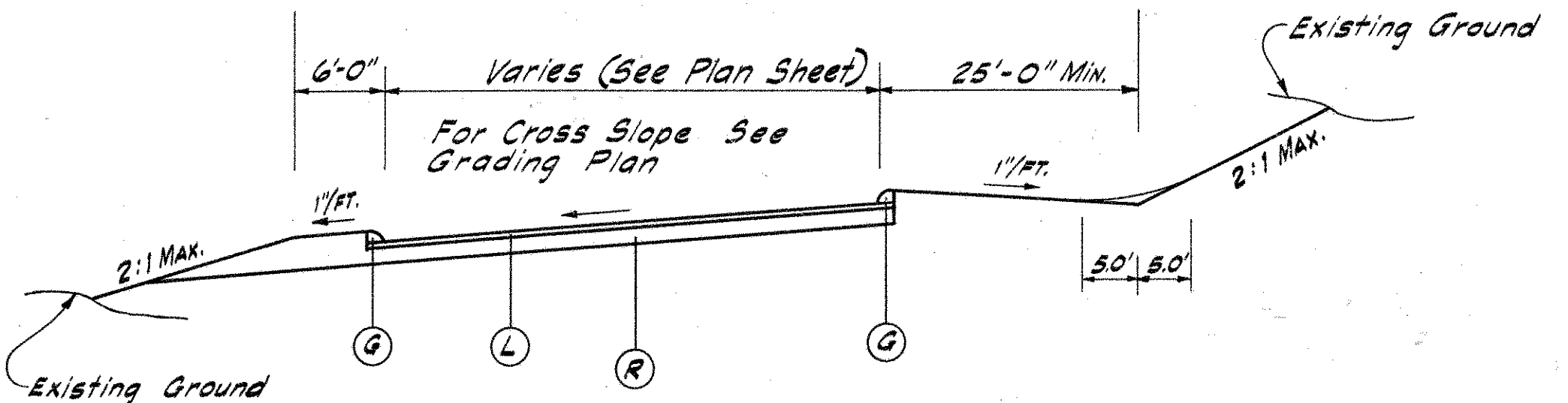
CONSTRUCTION OF
INTERSTATE ROUTE 691



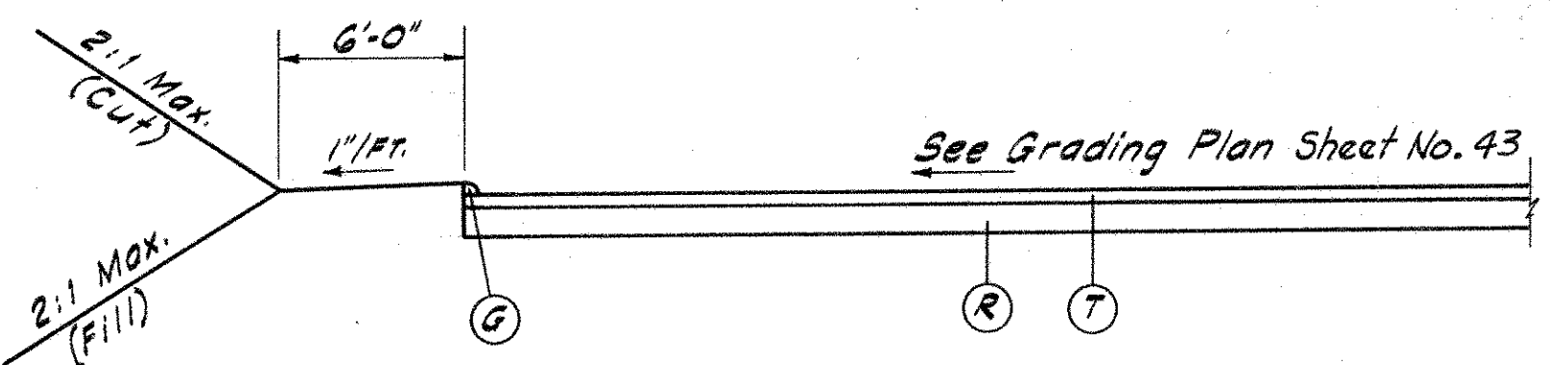
NORMAL SECTION

I-84 DETOUR A & B

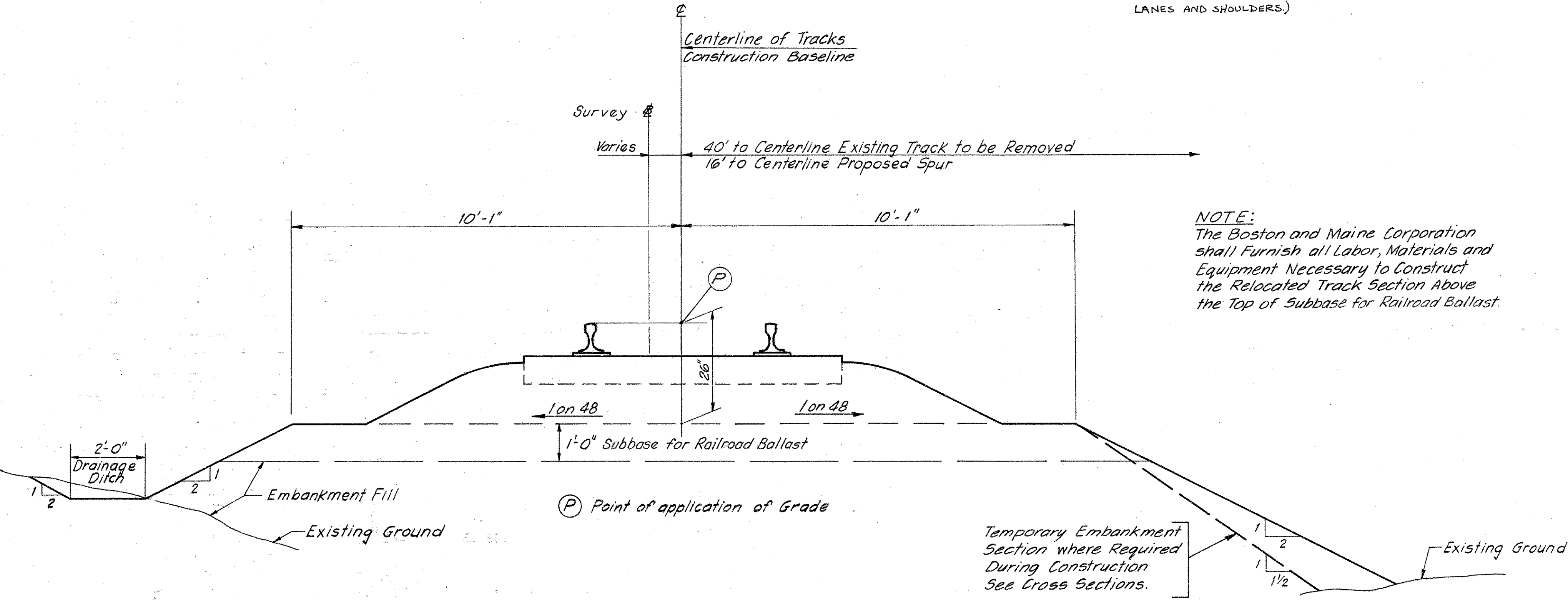
NOTE: A 1 1/2" Bituminous Concrete Class 1 Shall be placed where the detour crosses existing I-84 Eastbound or Westbound concrete pavement. This shall be removed when the detours are removed. (AN ADDITIONAL 1 1/2" OF BIT. CONC. CLASS 1 WAS PLACED TO PROVIDE A 3" OVERLAY OVER ALL THE EXISTING CONCRETE PAVEMENT ON E.B. AND W.B. I-84. 3/4" OF BIT. CONC. CLASS 14 WAS THEN PLACED ON ALL TRAFFIC LANES AND SHOULDERS.)



REST AREA ROADWAYS



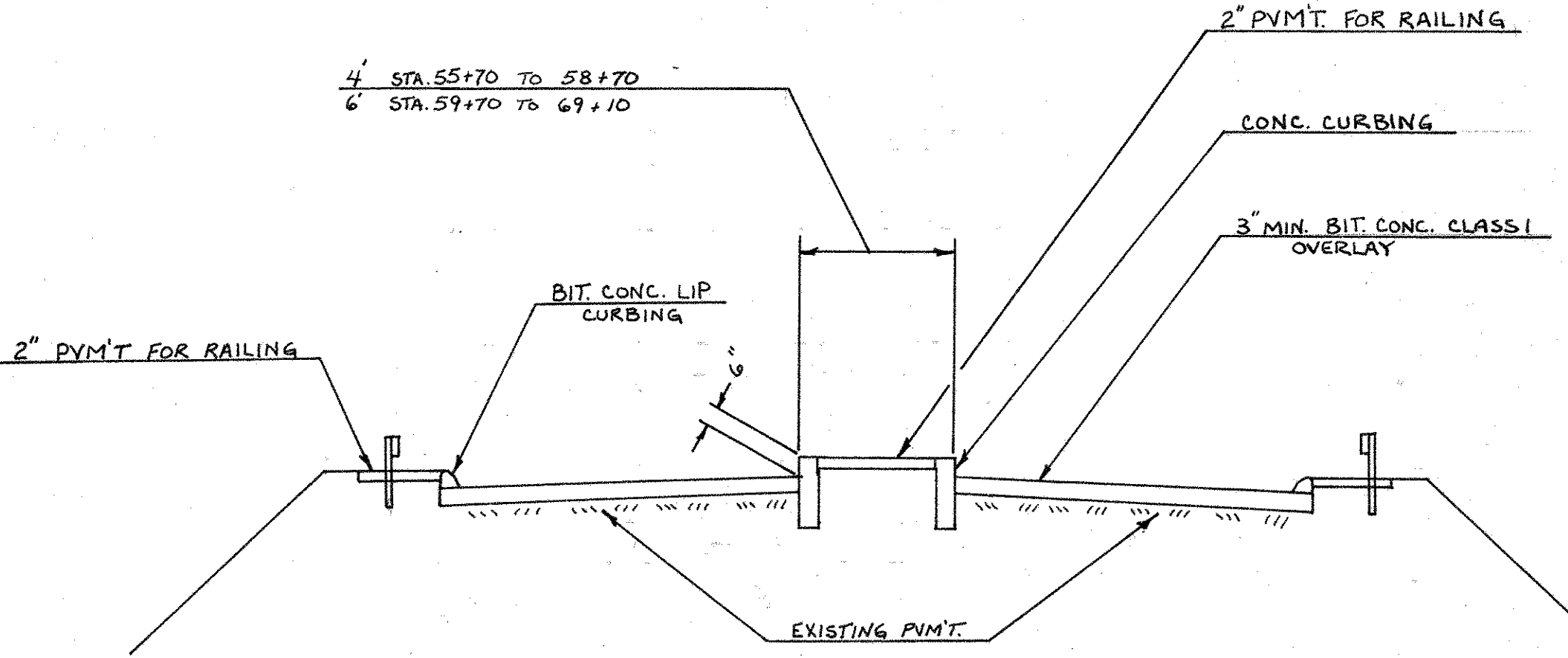
COMMUTER PARKING LOT



TYPICAL EMBANKMENT SECTION
(Looking North)

PROPOSED B & M TRACK RELOCATION

NOTE: The Boston and Maine Corporation shall furnish all Labor, Materials and Equipment Necessary to Construct the Relocated Track Section Above the Top of Subbase for Railroad Ballast.



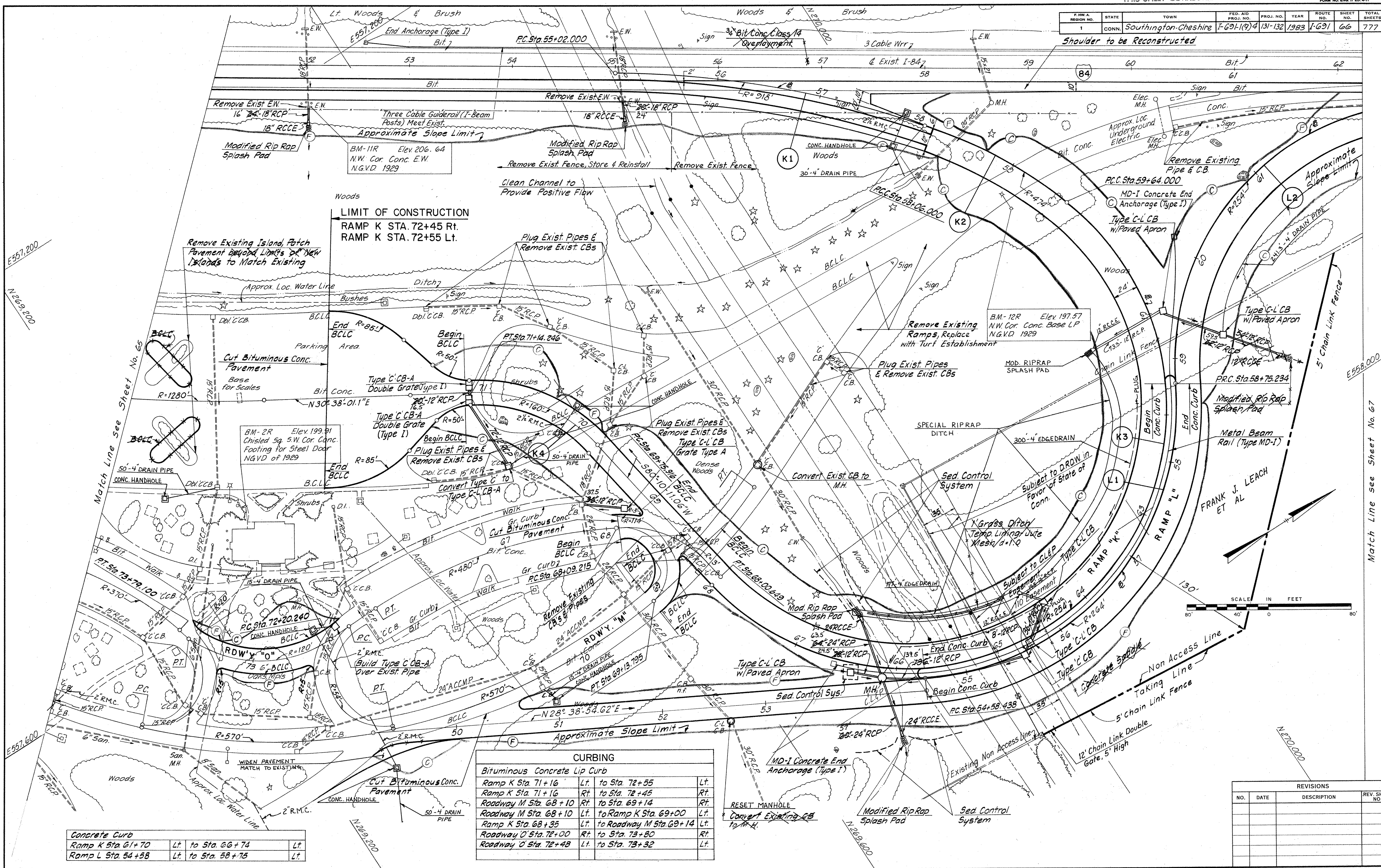
TYPICAL SECTION

ROUTE 322

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Southington-Cheshire	F-691-1(9)4	131-132	1983	I-691	66	777

PLAN	NO.	DATE	CHECKED BY	DATE	BY	DATE	BY	DATE	BY
Surveyed									
Plotted									
Traced									
Designed									
New Const. Noted									
Top. Field Chk. Noted									



Concrete Curb			
Ramp K Sta. 61+70	Lt.	to Sta. 66+74	Lt.
Ramp L Sta. 54+58	Lt.	to Sta. 58+75	Lt.

CURBING			
Bituminous Concrete Lip Curb			
Ramp K Sta. 71+16	Lt.	to Sta. 72+55	Lt.
Ramp K Sta. 71+16	Rt.	to Sta. 72+45	Rt.
Roadway M Sta. 68+10	Rt.	to Sta. 69+14	Rt.
Roadway M Sta. 68+10	Lt.	to Ramp K Sta. 69+00	Lt.
Ramp K Sta. 68+35	Lt.	to Roadway M Sta. 69+14	Lt.
Roadway O Sta. 72+00	Rt.	to Sta. 73+80	Rt.
Roadway O Sta. 72+48	Lt.	to Sta. 73+32	Lt.

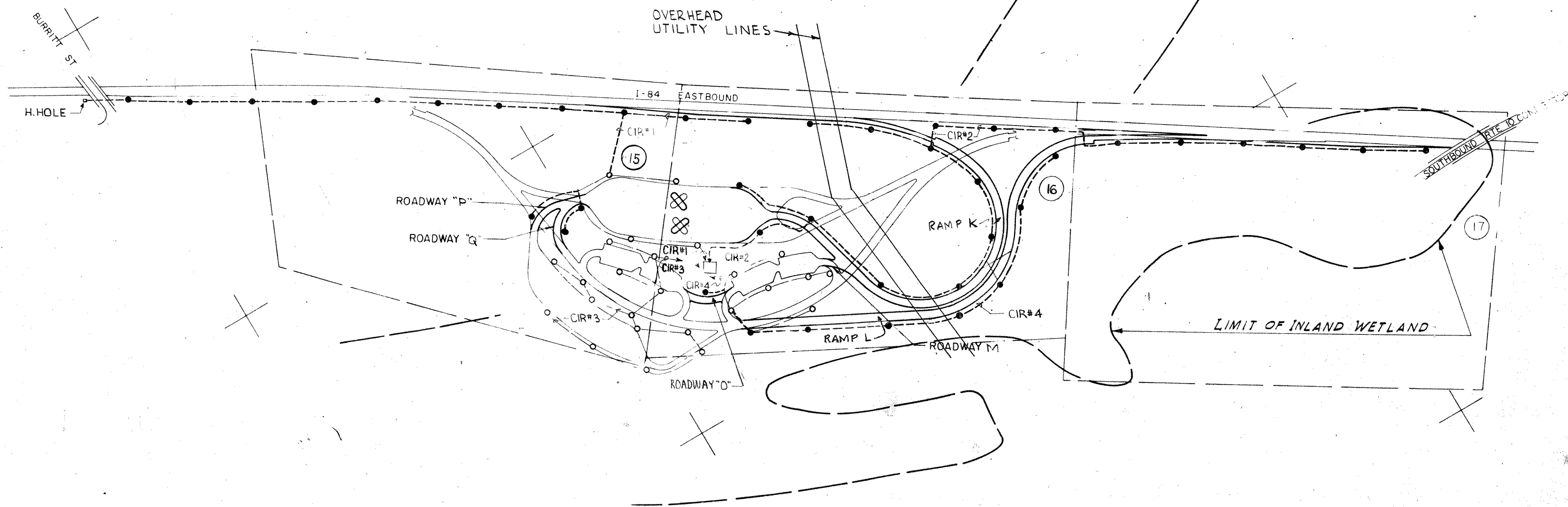
REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

PLAN	NO
Surveyed	
Plotted	
Traced	
Designed	
New Const. Noted	
Top. Field Chk. Noted	
L.G.	
T.E.	
NOTEBOOK	

F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Southington-Cheshire	I-691-119-4	131-132	1983	I-691	171A	777

SCALE IN FEET
200 0 200

INDEX PLAN



NOTES

- FOR THE REST AREA (1"=40FL SCALE)
LIGHTING LAYOUTS SEE DRAWING NOS
E-15, E-16 & E-17.
- FOR EXISTING REST AREA ILLUMINATION,
REFER TO PROJECT NO 131-102.

LEGEND (FOR THIS SHEET ONLY)

- RELOCATED OR PROPOSED LIGHT STANDARD
WITH 1 LUMINAIRE. (LAMP: 200 W OR 150 W
H.P.S. CLEAR)
- EXISTING LIGHT STANDARD TO
REMAIN-WITH NEW LUMINAIRE (LAMP:
200 W OR 150 W H.P.S. CLEAR)
- PROPOSED NON-METALLIC SHEATHED CABLE
(3#6) & #8 GROUND CONDUCTOR.
- EXISTING NON-METALLIC SHEATHED CABLE
(3#6) & #8 GROUND CONDUCTOR TO
REMAIN.

REST AREA LIGHTING LAYOUT-WITH 30 FT STANDARD POLES

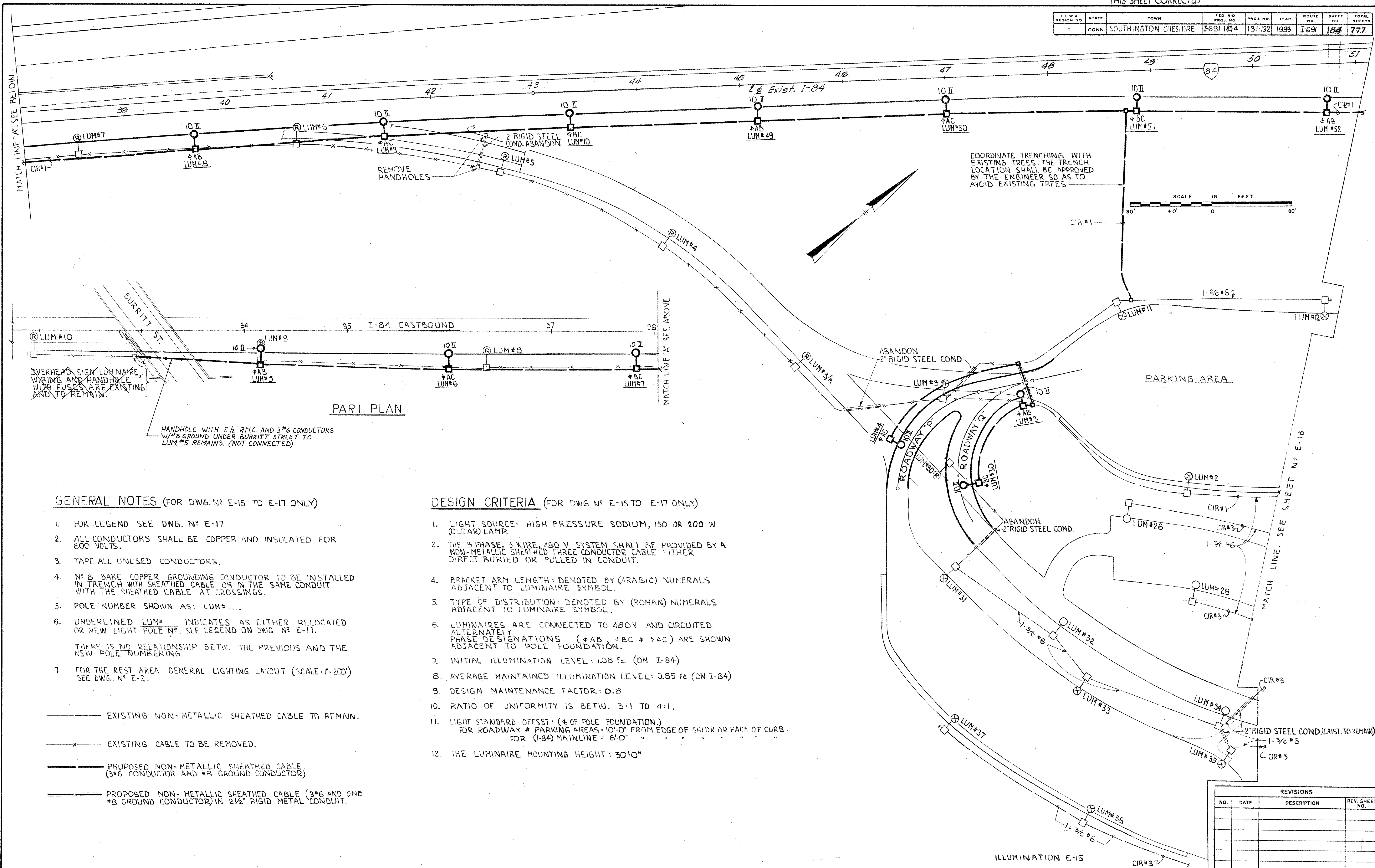
REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	11/86	Relocate Lum on Ramp L	171 A

ILLUMINATION E-2A

FOR INFORMATION ONLY

171A

F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON-CHESHIRE	I-691-104	131-132	1983	I-69	184	777



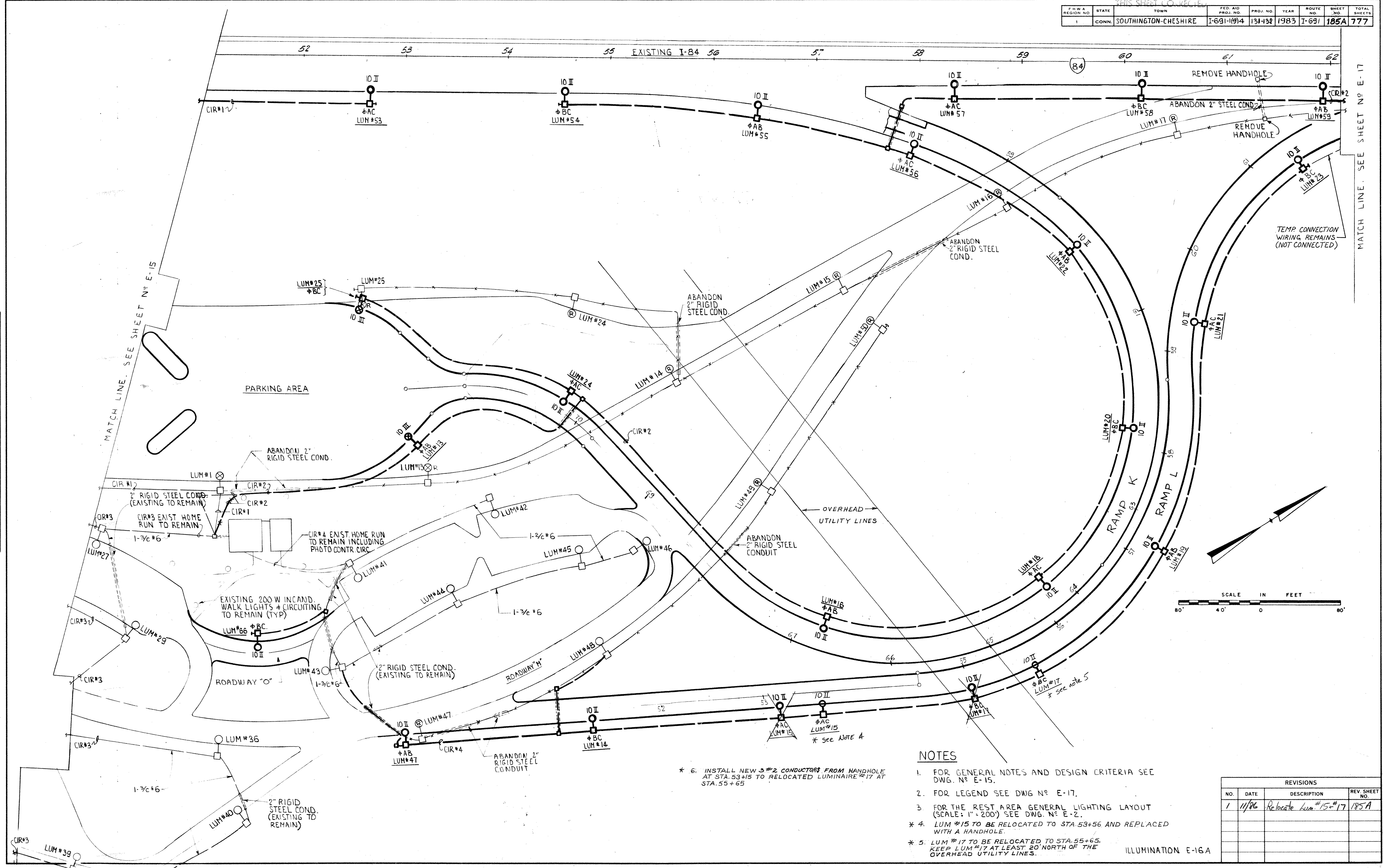
GENERAL NOTES (FOR DWG. NO. E-15 TO E-17 ONLY)

- FOR LEGEND SEE DWG. NO. E-17
- ALL CONDUCTORS SHALL BE COPPER AND INSULATED FOR 600 VOLTS.
- TAPE ALL UNUSED CONDUCTORS.
- NO. 8 BARE COPPER GROUNDING CONDUCTOR TO BE INSTALLED IN TRENCH WITH SHEATHED CABLE OR IN THE SAME CONDUIT WITH THE SHEATHED CABLE AT CROSSINGS.
- POLE NUMBER SHOWN AS: LUM#....
- UNDERLINED LUM# INDICATES AS EITHER RELOCATED OR NEW LIGHT POLE NO. SEE LEGEND ON DWG. NO. E-17.
THERE IS NO RELATIONSHIP BETW. THE PREVIOUS AND THE NEW POLE NUMBERING.
- FOR THE REST AREA GENERAL LIGHTING LAYOUT (SCALE: 1"=200') SEE DWG. NO. E-2.

- EXISTING NON-METALLIC SHEATHED CABLE TO REMAIN.
- X — EXISTING CABLE TO BE REMOVED.
- PROPOSED NON-METALLIC SHEATHED CABLE (3#6 CONDUCTOR AND #8 GROUND CONDUCTOR)
- PROPOSED NON-METALLIC SHEATHED CABLE (3#6 AND ONE #8 GROUND CONDUCTOR) IN 2 1/2" RIGID METAL CONDUIT.

DESIGN CRITERIA (FOR DWG. NO. E-15 TO E-17 ONLY)

- LIGHT SOURCE: HIGH PRESSURE SODIUM, 150 OR 200 W (CLEAR) LAMP.
- THE 3 PHASE, 3 WIRE, 480 V. SYSTEM SHALL BE PROVIDED BY A NON-METALLIC SHEATHED THREE CONDUCTOR CABLE EITHER DIRECT BURIED OR PULLED IN CONDUIT.
- BRACKET ARM LENGTH: DENOTED BY (ARABIC) NUMERALS ADJACENT TO LUMINAIRE SYMBOL.
- TYPE OF DISTRIBUTION: DENOTED BY (ROMAN) NUMERALS ADJACENT TO LUMINAIRE SYMBOL.
- LUMINAIRE ARE CONNECTED TO 480V AND CIRCUITED ALTERNATELY. PHASE DESIGNATIONS (+AB, +BC, +AC) ARE SHOWN ADJACENT TO POLE FOUNDATION.
- INITIAL ILLUMINATION LEVEL: 1.06 fc. (ON I-84)
- AVERAGE MAINTAINED ILLUMINATION LEVEL: 0.85 fc. (ON I-84)
- DESIGN MAINTENANCE FACTOR: 0.8
- RATIO OF UNIFORMITY IS BETW. 3:1 TO 4:1.
- LIGHT STANDARD OFFSET: (1/2 OF POLE FOUNDATION.)
FOR ROADWAY & PARKING AREAS: 10'-0" FROM EDGE OF SHLDR OR FACE OF CURB.
FOR (I-84) MAINLINE: 6'-0"
- THE LUMINAIRE MOUNTING HEIGHT: 30'-0"



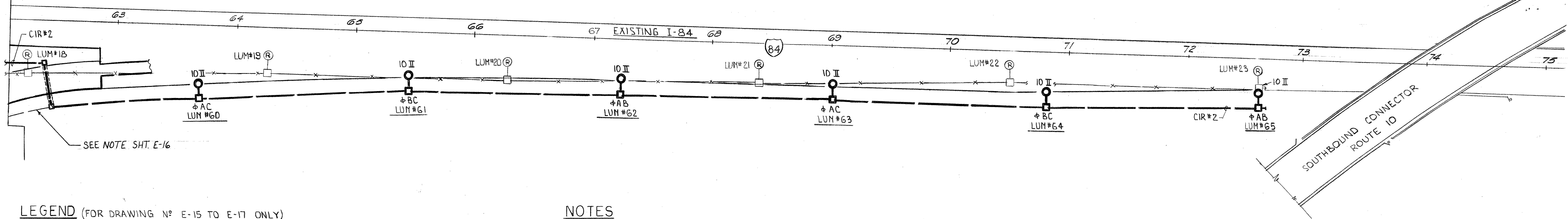
- NOTES**
- FOR GENERAL NOTES AND DESIGN CRITERIA SEE DWG. N° E-15.
 - FOR LEGEND SEE DWG N° E-17.
 - FOR THE REST AREA GENERAL LIGHTING LAYOUT (SCALE: 1"=200') SEE DWG. N° E-2.
 - LUM #15 TO BE RELOCATED TO STA. 53+56 AND REPLACED WITH A HANDHOLE.
 - LUM #17 TO BE RELOCATED TO STA. 55+65. KEEP LUM #17 AT LEAST 20' NORTH OF THE OVERHEAD UTILITY LINES.
 - INSTALL NEW 3/4" #2 CONDUCTORS FROM HANDHOLE AT STA. 53+15 TO RELOCATED LUMINAIRE #17 AT STA. 55+65.
- ILLUMINATION E-16A

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.
1	11/86	Relocate Lum #15 & #17	185A

THIS SHEET CORRECTED

F.H.W.A. REGION NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHINGTON-CHESHIRE	I-691-194	131-132	1983	I-691	186	777

MATCH LINE. SEE SHEET N° E-16

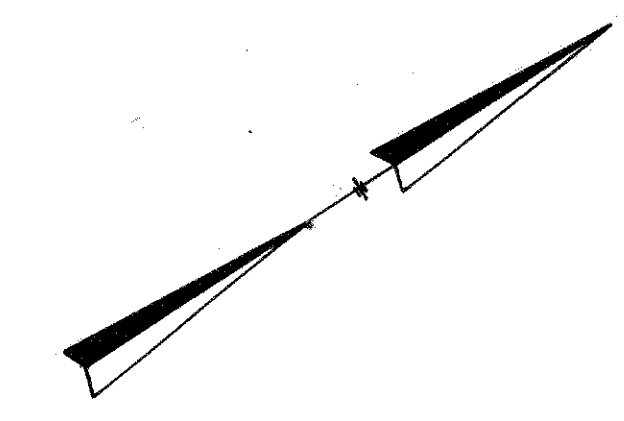
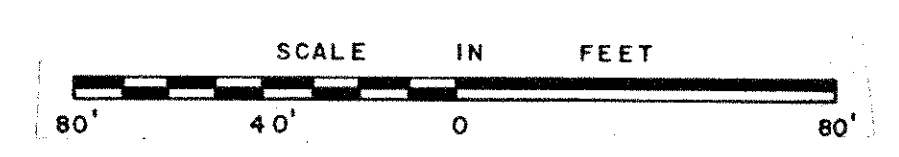


LEGEND (FOR DRAWING N° E-15 TO E-17 ONLY)

- (TYPE-1) EXISTING LIGHT STANDARD, CIRCUITING & LUM. N° TO REMAIN. THE EXISTING LUMINAIRE & BALLAST TO BE REPLACED WITH 150 W HPS LUMINAIRE WITH INTEGRAL BALLAST.
- (TYPE-2) EXISTING LIGHT STANDARD TO BE REMOVED AND RELOCATED (AS DESCRIBED UNDER TYPE-3)
- (TYPE-3) RELOCATED LIGHT STANDARD. (AS DESCRIBED UNDER TYPE-2) THE LUMINAIRE AND BALLAST TO BE REPLACED WITH 150 W HPS LUMINAIRE WITH INTEGRAL BALLAST.
OR:
PROPOSED LIGHT STANDARD WITH 150 W HPS LUMINAIRE AND CORRESPONDING BALLAST.
- (TYPE-4) EXISTING LIGHT STANDARD, CIRCUITING AND LUMINAIRE N° TO REMAIN. THE EXISTING LUMINAIRE AND BALLAST TO BE REPLACED WITH 200 W HPS LUMINAIRE WITH INTEGRAL BALLAST.
- (TYPE-5) EXISTING LIGHT STANDARD TO BE REMOVED AND RELOCATED. (AS DESCRIBED UNDER TYPE-6)
- (TYPE-6) RELOCATED LIGHT STANDARD. (AS DESCRIBED UNDER TYPE-5) THE EXISTING LUMINAIRE AND BALLAST TO BE REPLACED WITH 200 W HPS LUMINAIRE WITH INTEGRAL BALLAST.
- EXISTING NON-METALLIC SHEATHED CABLE TO REMAIN.
- EXISTING CABLE TO BE REMOVED.
- PROPOSED NON-METALLIC SHEATHED CABLE (3#6 CONDUCTOR AND #8 GROUND CONDUCTOR)
- PROPOSED NON-METALLIC SHEATHED CABLE (3#6 AND ONE #8 GROUND CONDUCTOR) IN 2 1/2" RIGID METAL CONDUIT.
- EXISTING HANDHOLE.
- PROPOSED CONCRETE HANDHOLE.
- CONDUIT END WITH CAP AT LIGHT STANDARD FOUNDATION.

NOTES

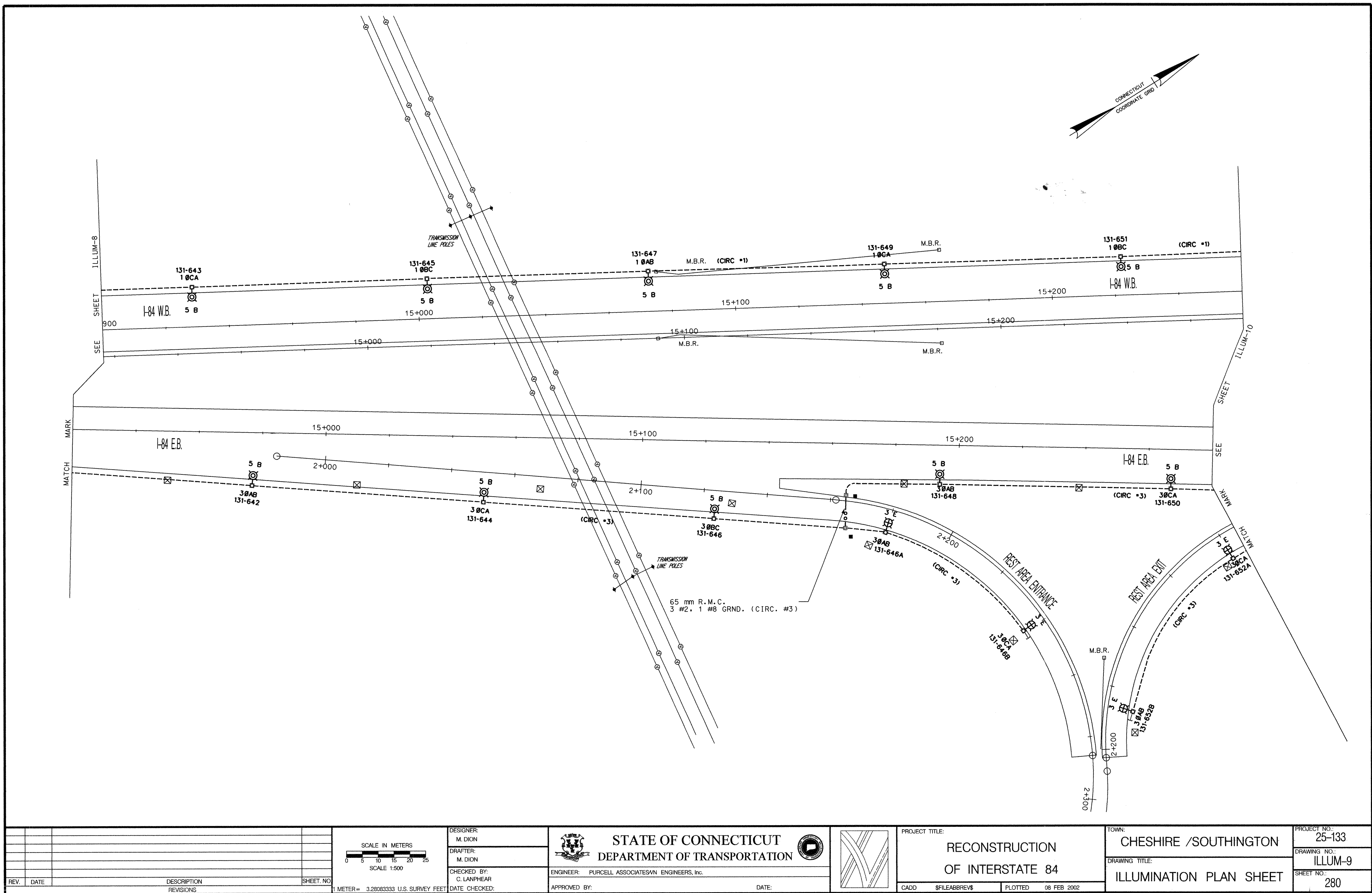
- FOR GENERAL NOTES AND DESIGN CRITERIA SEE DWG N° E-15.
- FOR THE REST AREA GENERAL LIGHTING LAYOUT (SCALE: 1"=200') SEE DWG N° E-2.



Surveyed	Plotted	Traced	Designed	New Const. Noted	Top. Field Chk. Noted
PLAN					
NOTEBOOK					
NO.					

REVISIONS			
NO.	DATE	DESCRIPTION	REV. SHEET NO.

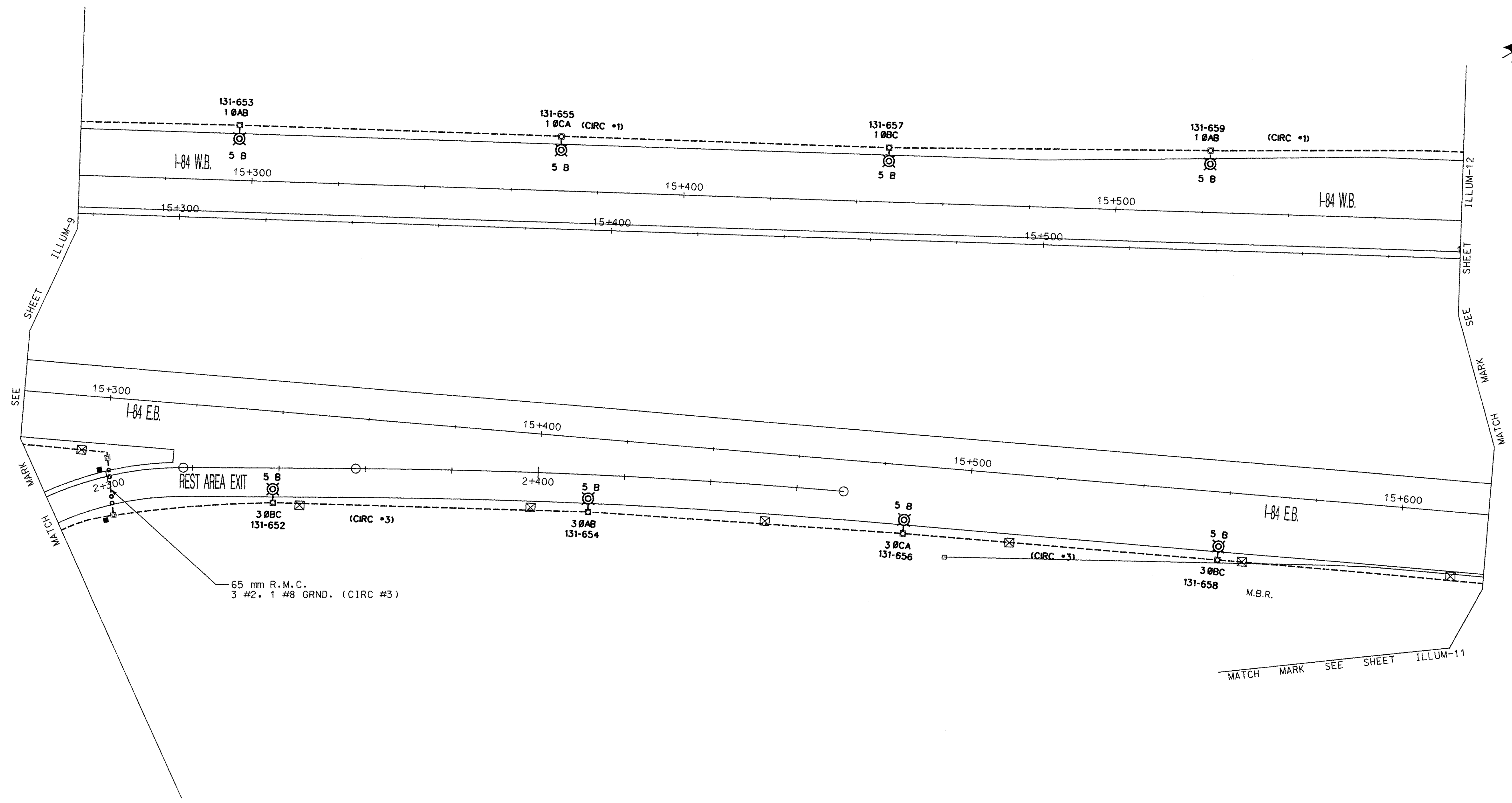
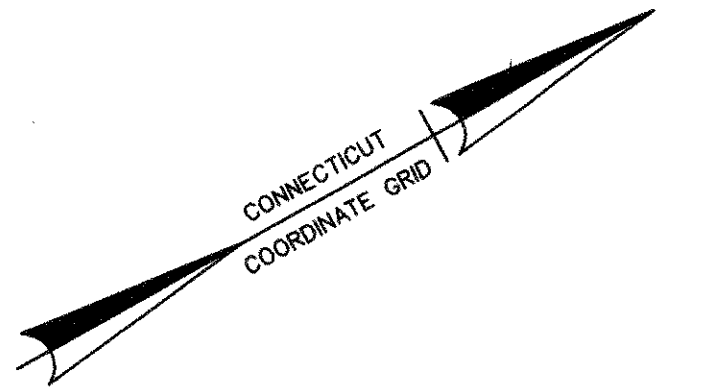
ILLUMINATION E-17



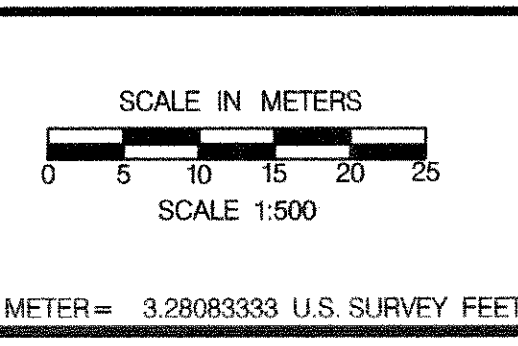
840-01

FOR INFORMATION ONLY

THIS SHEET NOT CORRECTED



REV.	DATE	DESCRIPTION	SHEET NO.

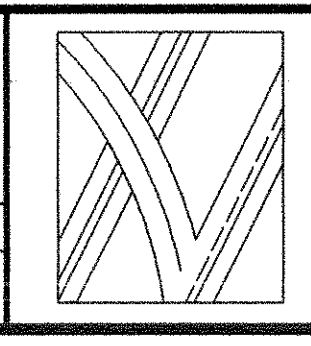


DESIGNER:
M. DION
DRAFTER:
M. DION
CHECKED BY:
C. LANPHEAR
DATE CHECKED:



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

ENGINEER: PURCELL ASSOCIATES/IN ENGINEERS, INC.
APPROVED BY: _____ DATE: _____



PROJECT TITLE:
**RECONSTRUCTION
OF INTERSTATE 84**
CADD \$FILEABBREV\$ PLOTTED 08 FEB 2002

TOWN:
CHESHIRE /SOUTHINGTON
DRAWING TITLE:
ILLUMINATION PLAN SHEET

PROJECT NO.:
25-133
DRAWING NO.:
ILLUM-10
SHEET NO.:
281

840-01

THIS SHEET NOT CORRECTED