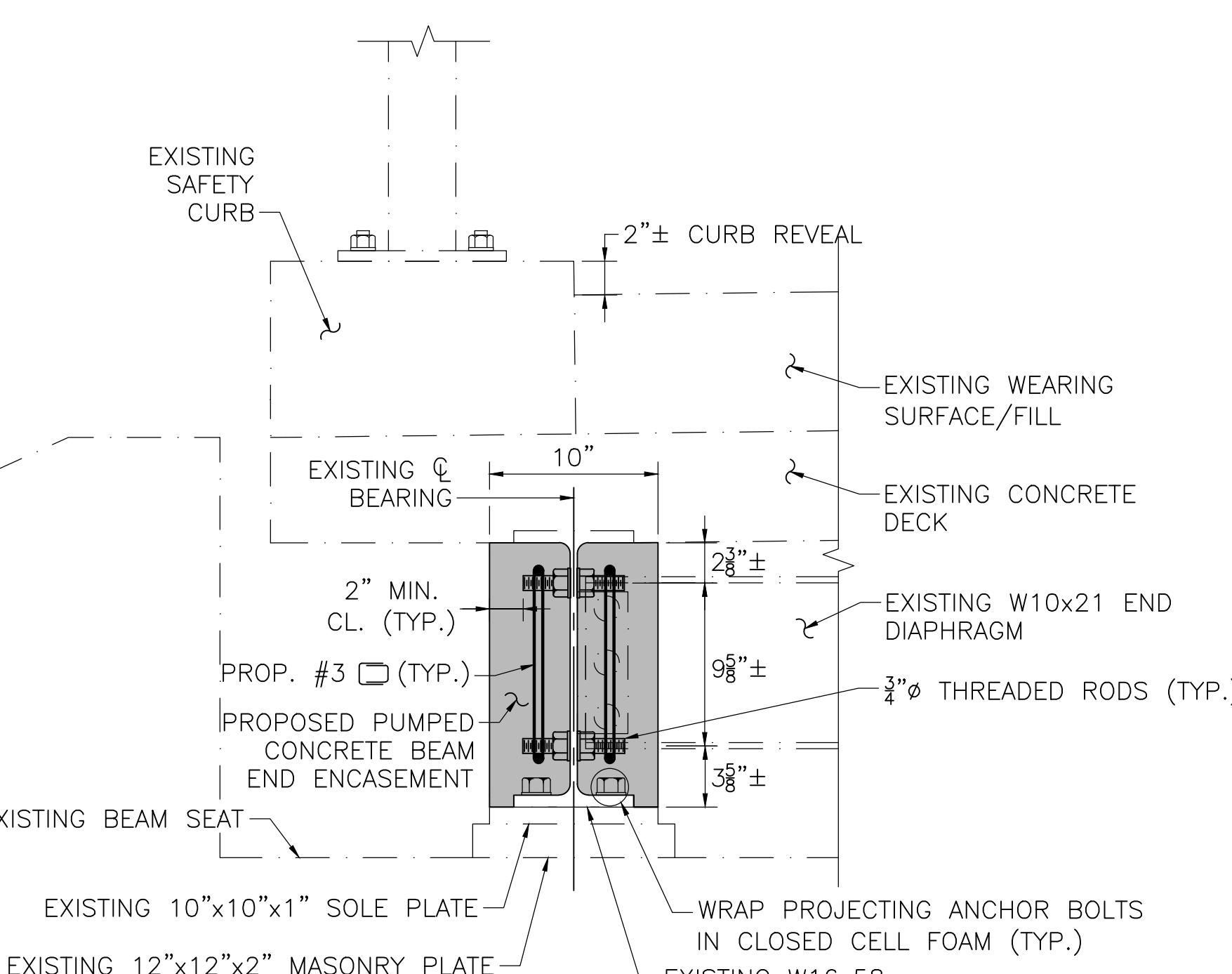


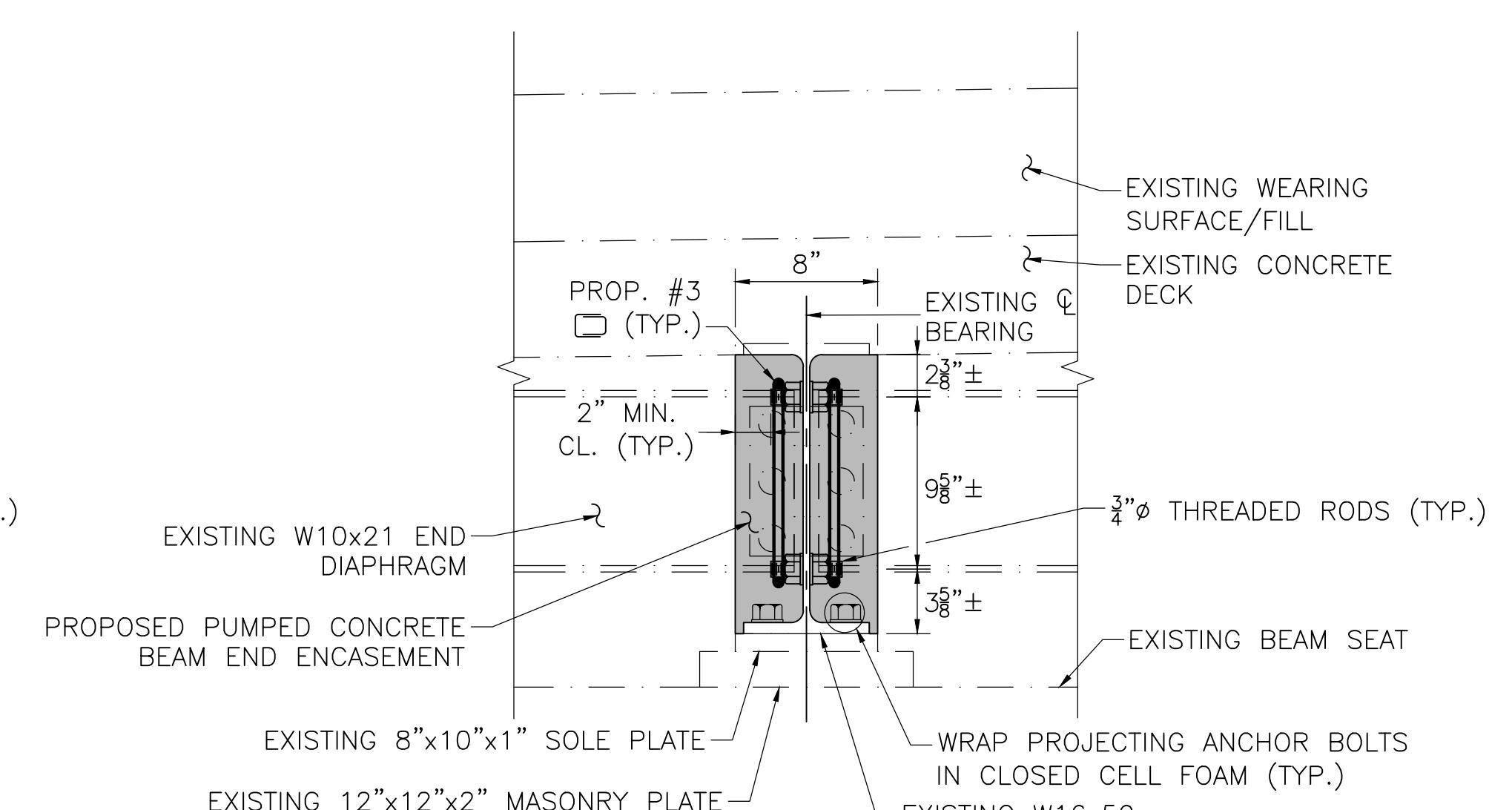
FRAMING PLAN

SCALE: $\frac{3}{16}$ " = 1'-0"



FASCIA BEAM TYPICAL SECTION

SCALE: $\frac{1}{16}$ " = 1'-0"



INTERIOR BEAM TYPICAL SECTION

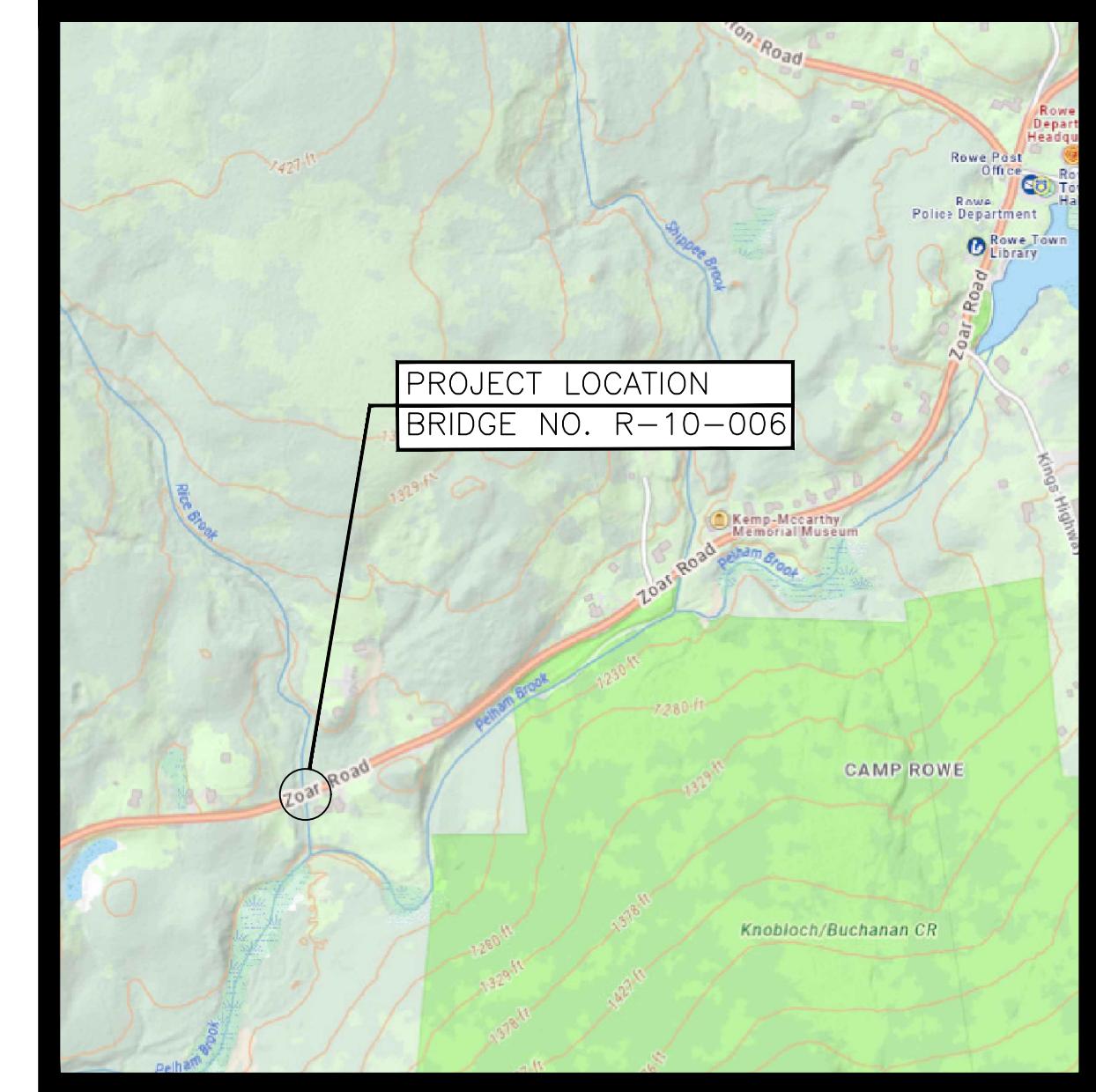
SCALE: $\frac{1}{16}$ " = 1'-0"

GENERAL NOTES

- GENERAL DESIGN IN ACCORDANCE WITH THE 17TH EDITION OF THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES WITH INTERIM SPECIFICATIONS THROUGH 2002, FOR H20 LOADING.
- ALL WORK SHALL BE IN ACCORDANCE WITH THE RELEVANT PROVISIONS OF THE 2025 MASSDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES WITH THE LATEST UPDATES AND SUPPLEMENTAL SPECIFICATIONS.
- THESE DRAWINGS WERE FORMATTED FOR 24"x36" PLAN SHEETS. SCALES NOTED ON THE DRAWINGS ARE NOT APPLICABLE TO REDUCED SIZE PRINTS.
- EXISTING STRUCTURE DIMENSIONS AND DETAILS ARE ACCORDING TO ORIGINAL CONSTRUCTION DRAWINGS, OBSERVATION, AND FIELD MEASUREMENTS AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL DETERMINE AND ESTABLISH ALL DIMENSIONS AND EXISTING DETAILS NECESSARY FOR COMPLETION OF ALL WORK BY FIELD MEASUREMENT (I.E. BEARING LOCATIONS, EXISTING REPAIRS, ETC.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ADEQUACY AND ACCURACY THEREOF, AND SHALL NOT ORDER ANY MATERIAL OR COMMENCE ANY FABRICATION UNTIL THEY HAVE MADE THE REQUIRED MEASUREMENTS ON THE ACTUAL STRUCTURE AND THE EXTENT OF THE PROPOSED WORK HAS BEEN APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY NOT TO DAMAGE UTILITIES, EQUIPMENT, STRUCTURES, OR THOSE PORTIONS OF THE SUPERSTRUCTURE THAT ARE TO REMAIN IN PLACE, THAT ARE TO BE REUSED, OR THAT ARE TO REMAIN THE PROPERTY OF OTHERS. ANY PORTION OF THE SUPERSTRUCTURE AND SUBSTRUCTURE TO REMAIN WHICH BECOMES DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AND AT THE CONTRACTOR'S EXPENSE.
- REINFORCING STEEL SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M31 GRADE 60.
- THREADED RODS SHALL BE $\frac{3}{8}$ " AND CONFORM TO ASTM A307 GRADE A. NUTS AND WASHERS SHALL CONFORM TO AASHTO M291 AND M293 RESPECTIVELY.
- NO REPAIRS SHALL COMMENCE UNTIL ALL MEASUREMENTS HAVE BEEN MADE AND MATERIALS (EXCLUDING CONCRETE) ARE ON HAND FOR THE REPAIR.
- THE CONTRACTOR SHALL ASSUME ALL EXISTING COATINGS CONTAIN LEAD UNLESS OTHERWISE DETERMINED BY TESTING. ALL DEBRIS CREATED BY OPERATIONS SHALL BE PROPERLY CONTAINED AND DISPOSED OF.
- A MASSDOT APPROVED NON-SHRINK GROUT MAY BE SUBSTITUTED FOR THE 5000 PSI $\frac{3}{8}$ INCH CONCRETE TO FACILITATE DELIVERY, FULL ENCASEMENT, AND CONSOLIDATION.

CONSTRUCTION PROCEDURE

- THE EXISTING CONCRETE BEAM SEATS, BACKWALLS, AND STRUCTURAL STEEL SHALL BE CLEANED PRIOR TO PLACEMENT OF THE BEAM END ENCASEMENT. ALL DEBRIS AND DELETERIOUS MATERIAL SHALL BE REMOVED FROM THE BEAM SEAT. THE EXISTING STRUCTURAL STEEL SHALL BE POWER TOOL CLEANED TO REMOVE ALL LOOSE RUST AND PAINT. TIGHTLY ADHERING PAINT DOES NOT NEED TO BE REMOVED.
- PRIOR TO PLACEMENT OF THE BEAM END ENCASEMENT, EXPANDED POLYSTYRENE SHALL BE ATTACHED WITH ADHESIVE TO ALL SURFACES OF THE BACKWALLS AND BEAM SEATS. THE CONTRACTOR SHALL INSURE THAT ALL THE EXISTING ABUTMENT CONCRETE IS PROPERLY LINED WITH EXPANDED POLYSTYRENE SO THAT THE ENCASEMENT CONCRETE DOES NOT COME IN CONTACT WITH EXISTING ABUTMENT CONCRETE.
- CEMENT CONCRETE SHALL BE 5000 PSI, $\frac{3}{8}$ ", 710 HP CEMENT CONCRETE WITH A 7" ± 1 " SLUMP FROM A MASSDOT APPROVED PRODUCER. MIX DESIGN SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO PLACEMENT.
- CEMENT CONCRETE CONSOLIDATION SHALL BE ACHIEVED USING CONCRETE PENCIL VIBRATORS THAT ARE INSERTED THRU OPENINGS IN THE TOP OF FORMS. A MINIMUM OF TWO CONCRETE PENCIL VIBRATORS SHALL BE ON-SITE DURING CONCRETE PLACEMENT.
- THE CONTRACTOR'S MEANS AND METHODS OF CONTAINING AND DISPOSING DEBRIS AND OBTAINING ACCESS TO THE BEAM ENDS TO BE ENCASED SHALL BE CONSIDERED INCIDENTAL TO THE WORK WITHOUT SEPERATE COMPENSATION.



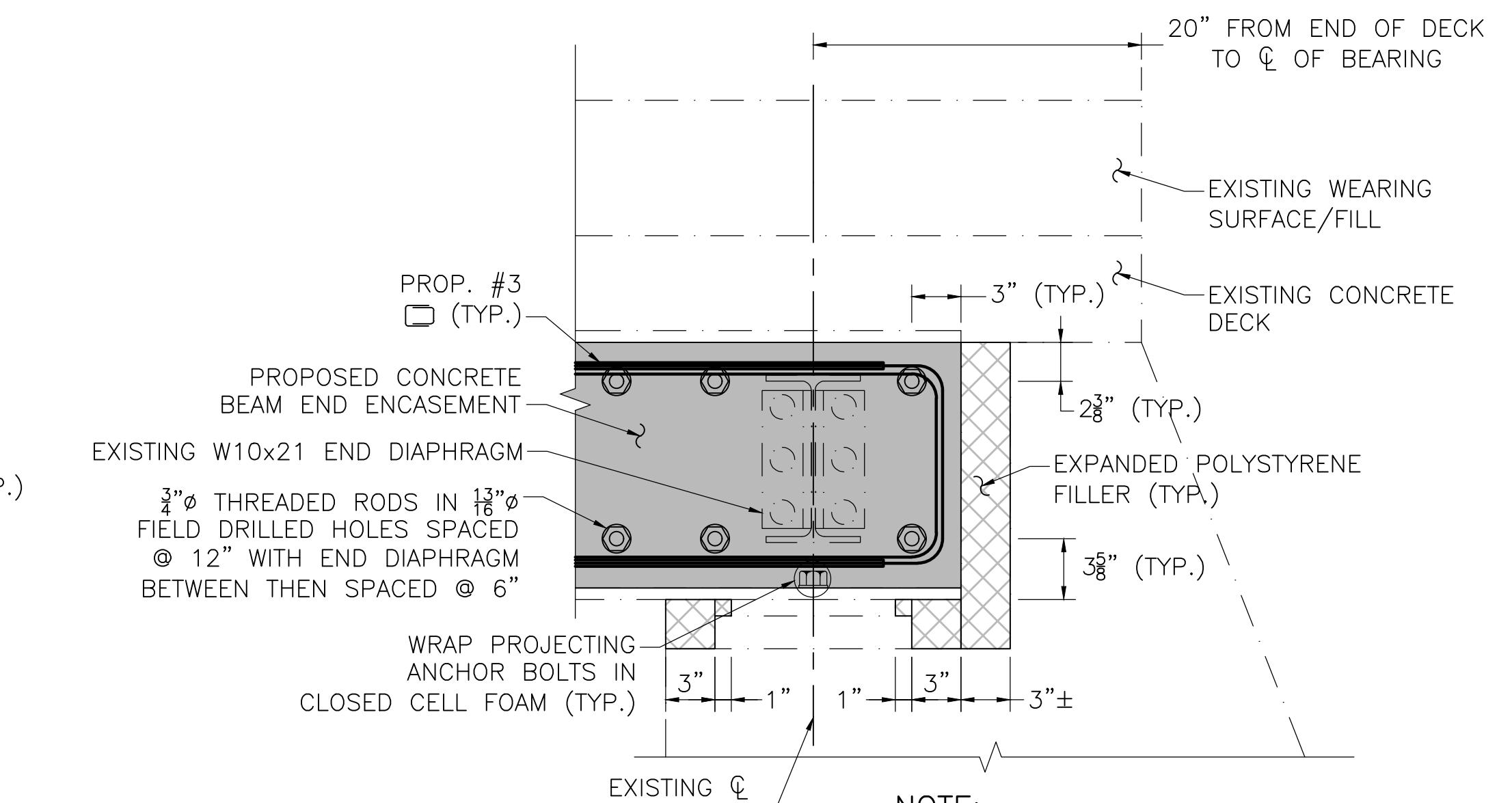
LOCUS
NOT TO SCALE

ESTIMATED QUANTITIES
NOT GUARANTEED

ITEM NO.	ITEM	UNIT	QUANTITY
905.2	5000 PSI, 3/8 INCH, 710 HP CEMENT CONCRETE	CY	1
910.1	STEEL REINFORCEMENT FOR REPAIRS	LB	70

NOTE:

ORIGINAL BRIDGE PLANS INDICATE A 10" CURB REVEAL. THE ROUTINE INSPECTION REPORT DATED AUGUST 2023 REPORT DOCUMENTS A 2" CURB REVEAL. THIS SUGGESTS AN ADDITIONAL 8" OF WEARING SURFACE HAS BEEN ADDED TO THE BRIDGE SINCE ORIGINAL CONSTRUCTION.



NOTE:
EXTERIOR BEAM SHOWN INTERIOR SIMILAR

TYPICAL LONGITUDINAL SECTION

SCALE: $\frac{1}{16}$ " = 1'-0"

COMMONWEALTH OF MASSACHUSETTS
MassDOT, Highway Division
APPROVED UNDER PROVISIONS OF
MASS. GEN. LAWS CH 85 S 35
10/21/2025
STATE BRIDGE ENGINEER
DATE

BEAM END ENCASEMENT REPAIRS
TOWN OF ROWE
BRIDGE REPAIRS FOR ROWE R-10-006
ZOAR ROAD OVER SAM RICE BROOK

GENERAL NOTES AND REPAIR DETAILS
SHEET 1 OF 1

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GILL
ENGINEERING