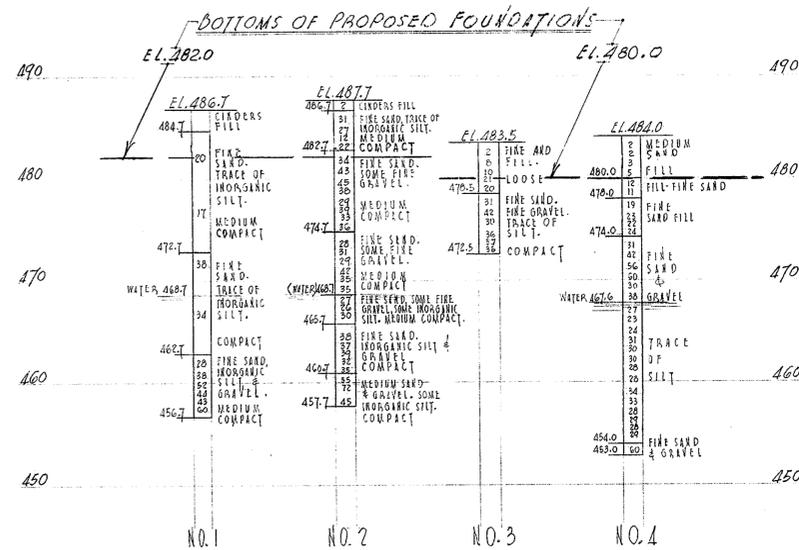


PLAN  
SCALE 1"=40'-0"

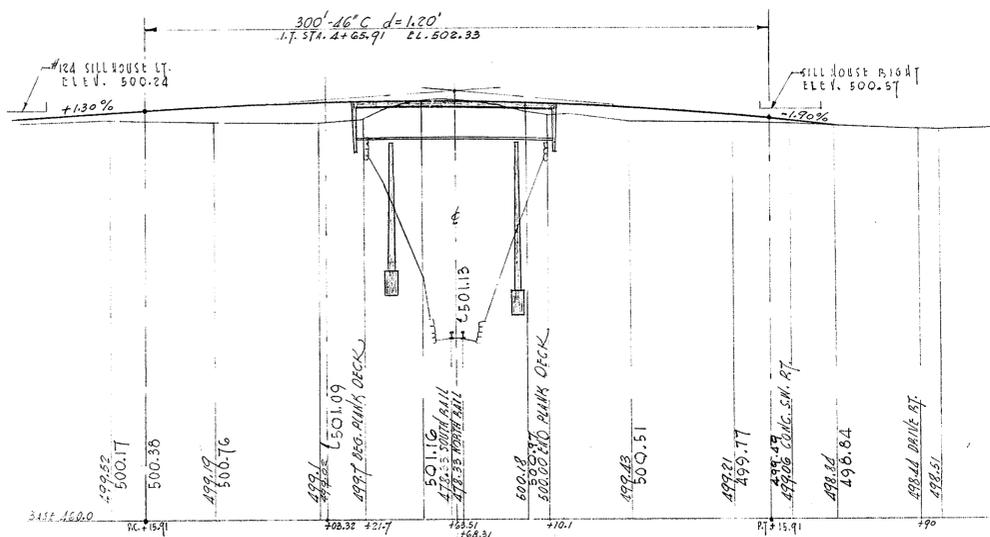


BORING DATA

BORINGS TAKEN BY AMERICAN DRILLING CO. APR. 1955. SCALE 1"=8'-0"

BORING NOTES

LOCATIONS OF BORINGS SHOWN ON KEY PLAN TABS: NO. 1. BORINGS TAKEN FOR PURPOSE OF DESIGN AND SHOW CONDITIONS AT BORING POINTS ONLY, BUT DO NOT NECESSARILY SHOW NATURE OF MATERIALS TO BE ENCOUNTERED DURING CONSTRUCTION. FIGURES IN COLUMNS INDICATE BLOWS PER FOOT ON PIPE PRODUCED BY 30" FALL OF 140 POUND HAMMER. BORING SAMPLES MAY BE SEEN AT THE OFFICE OF THE BRIDGE ENGINEER, MR. JOHN C. RUNDLETT, ROOM NO. 609.



PROFILE OF LOG PLAIN ROAD

HORIZONTAL SCALE 1"=40'-0" VERTICAL SCALE 1"=8'-0"

GENERAL NOTES

FOUNDATIONS: MAY BE ALTERED, IF NECESSARY, TO SUIT CONDITIONS ENCOUNTERED IN CONSTRUCTION.

DATE: TO BE PLACED IN CENTRE OF INSIDE FACE OF NORTHWESTERLY AND SOUTHEASTERLY END POSTS, AS SHOWN IN DETAIL ON SHEET NO. 3. A SHEET SHOWING SIZE AND CHARACTER OF NUMERALS WILL BE FURNISHED.

DESIGN: ACCORDING TO SPECIFICATIONS OF THE AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS (1953 EDITION) FOR A15-A4 LOADING.

BENCH MARKS: D.M. NO. 1, STA. 1+55, 26' RIGHT. TWIN 12" x 15" OAK, RAILROAD SPIKE, EL. 500.00 (ASSUMED).

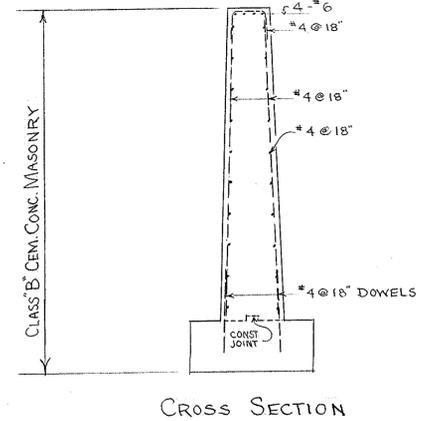
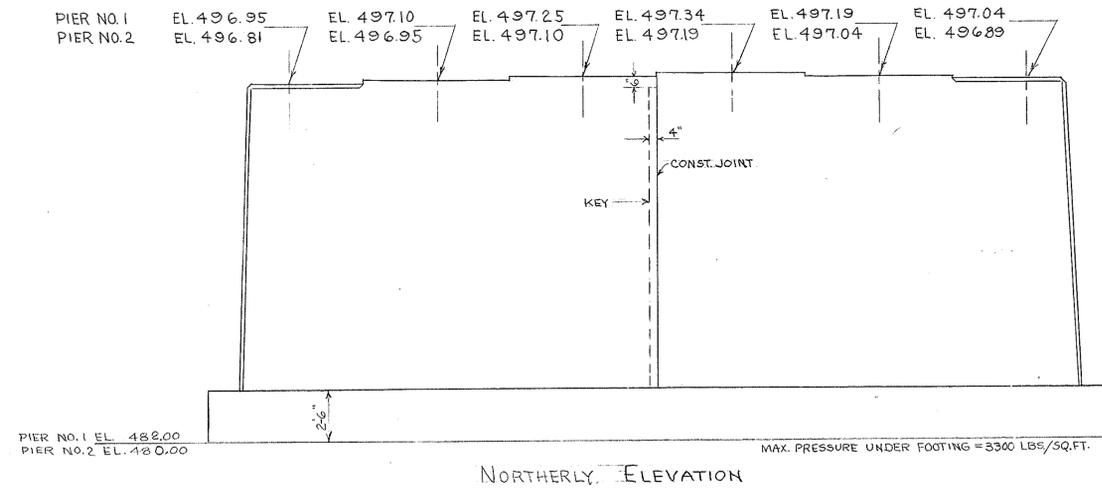
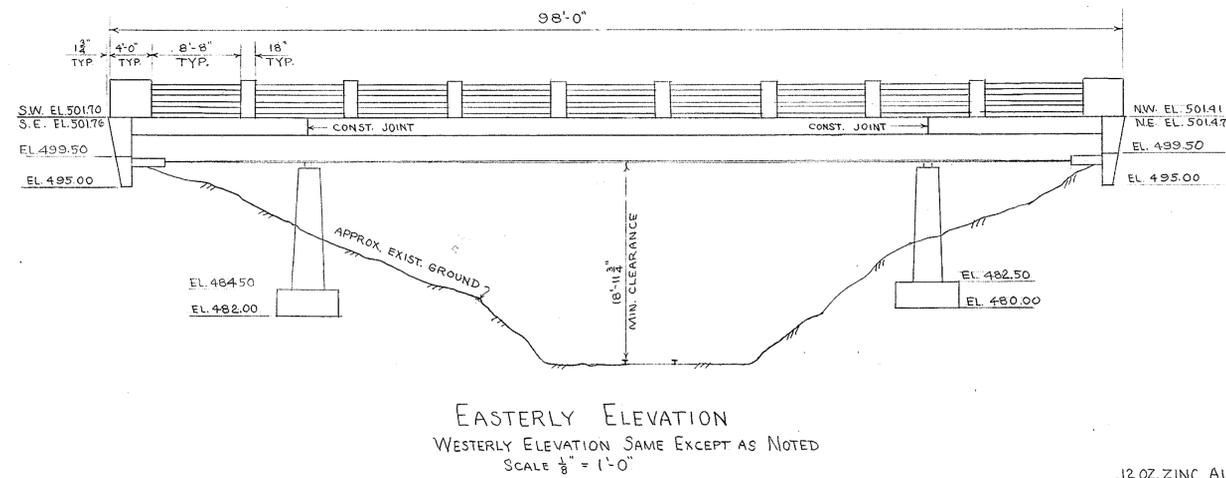
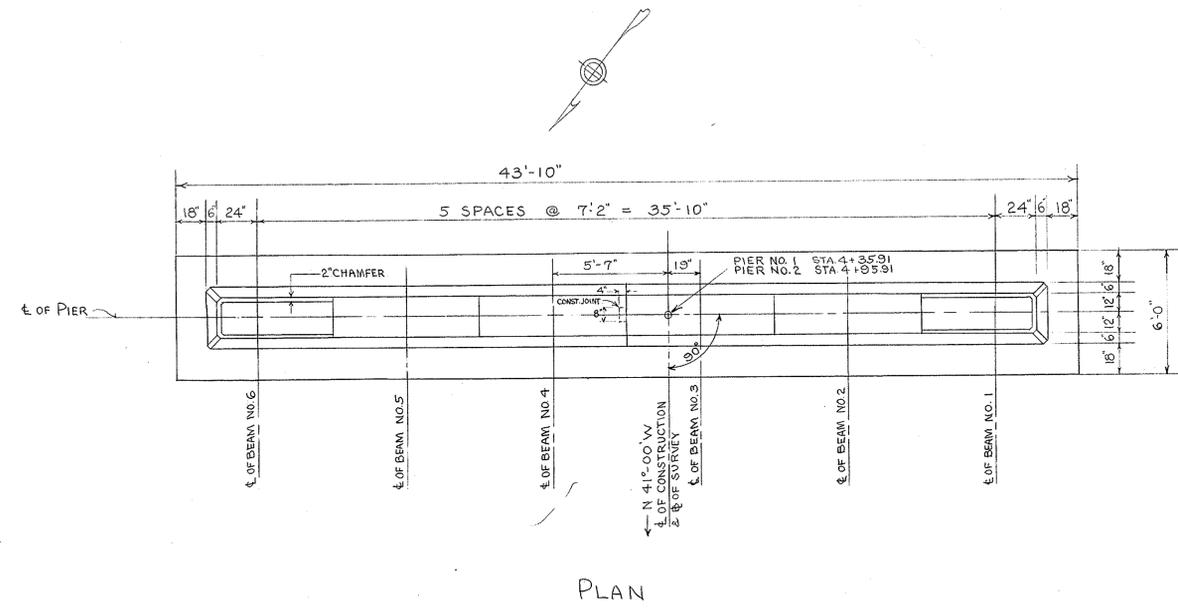
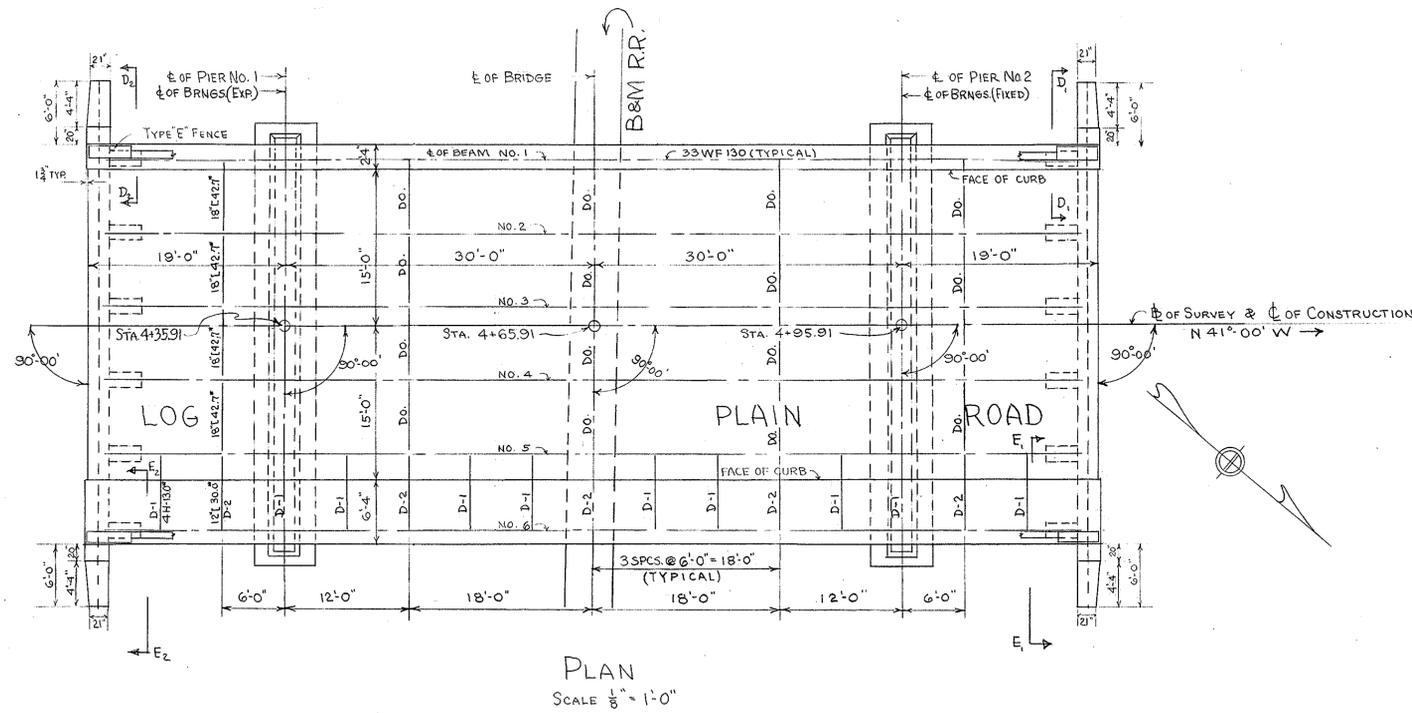
REINFORCEMENT: ALL BARS SHALL HAVE DEFORMATIONS CONFORMING TO A.S.T.M. DESIGNATION A305. UNLESS OTHERWISE SHOWN ON THE PLANS, REINFORCING BARS SHALL BE LAPPED 20 DIAMETERS TO MAKE A SPLICE, EXCEPT THAT MAIN REINFORCING BARS NEAR THE TOP OF SLABS AND BEAMS HAVING MORE THAN 12" OF CONCRETE UNDER THE BARS SHALL BE LAPPED 36 DIAMETERS TO MAKE A SPLICE.

ESTIMATED QUANTITIES

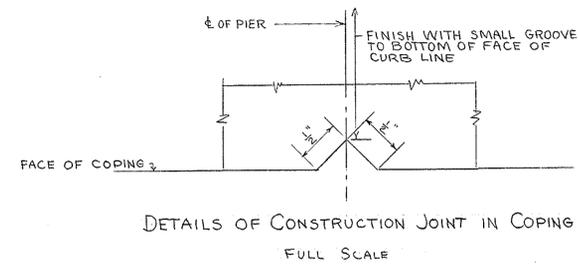
NOT GUARANTEED

BRIDGE EXCAVATION	320 C.Y.
CLASS B ROCK EXCAVATION	5 C.Y.
GRAVEL BORROW	45 C.Y.
REMOVAL OF PRESENT SUPERSTRUCTURE	1 L.S.
BRIDGE STRUCTURE	1 L.S.

DES. HOLDEN	SEPT. 8, 1956	DECIDED FOR CONSTRUCTION
DR. HOLDEN	THE COMMONWEALTH OF MASSACHUSETTS	
CHK. MCDONAGH	PROPOSED BRIDGE	
APPROVED FOR DES. ARCH. SPECS.	GREENFIELD	
	LOG PLAIN ROAD, STATION 4+65.91	
	OVER BOSTON & MAINE RAILROAD	
	OFFICE OF PUBLIC WORKS, 100 NASHUA ST., BOSTON, MASS. SEPT. 1956	
	<i>J. D. Rundlett</i>	<i>H. S. Long</i>
	BRIDGE ENGINEER	CIVIL ENGINEER

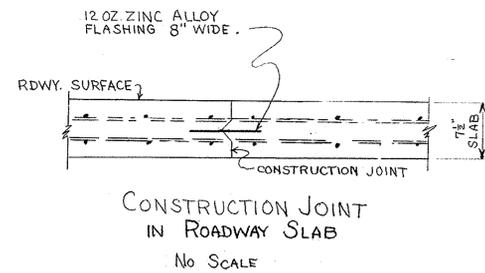


**TYPICAL PIER DETAILS**  
SCALE  $\frac{1}{4}'' = 1'-0''$

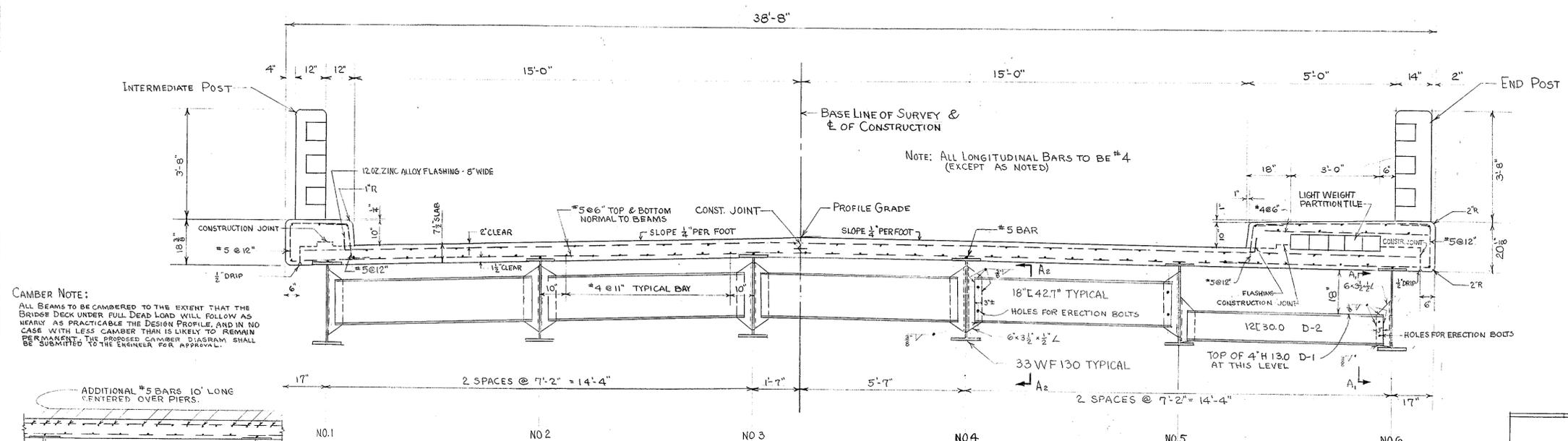


**SLAB POURING SEQUENCE**

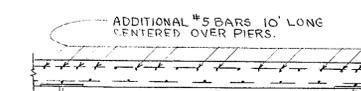
PLACE CONCRETE ON END SPANS SIMULTANEOUSLY AND CONTINUE ON TO CENTER SPAN 10'± BEYOND PIERS. CONCRETE ON CENTER SPAN SHALL BE PLACED NO SOONER THAN 48 HOURS AFTER PLACING OF CONCRETE ON END SPANS.



SEPT. 8, 1956	ISSUED FOR CONSTRUCTION
DATE	DESCRIPTION
USE ONLY PRINTS OF LATEST DATE	

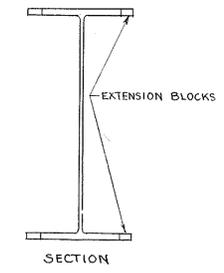
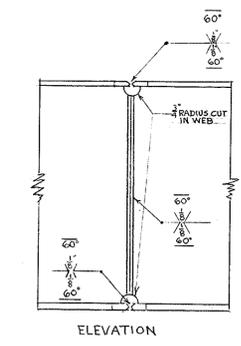
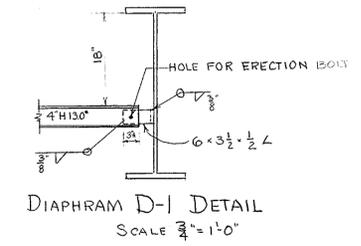
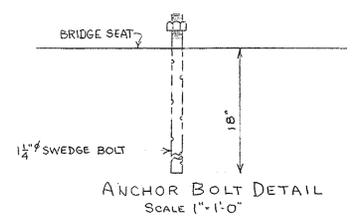
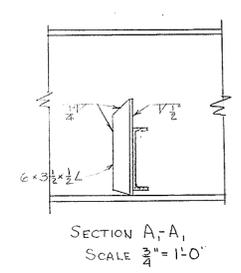
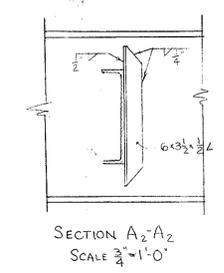


**CAMBER NOTE:**  
ALL BEAMS TO BE CAMBERED TO THE EXTENT THAT THE BRIDGE DECK UNDER FULL DEAD LOAD WILL FOLLOW AS NEARLY AS PRACTICABLE THE DESIGN PROFILE, AND IN NO CASE WITH LESS CAMBER THAN IS LIKELY TO REMAIN PERMANENT. THE PROPOSED CAMBER DIAGRAM SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

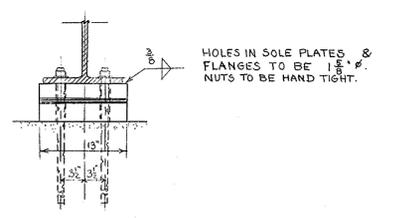
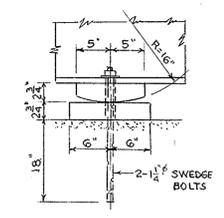


SLAB REINFORCEMENT OVER PIERS  
SCALE 1/2" = 1'-0"

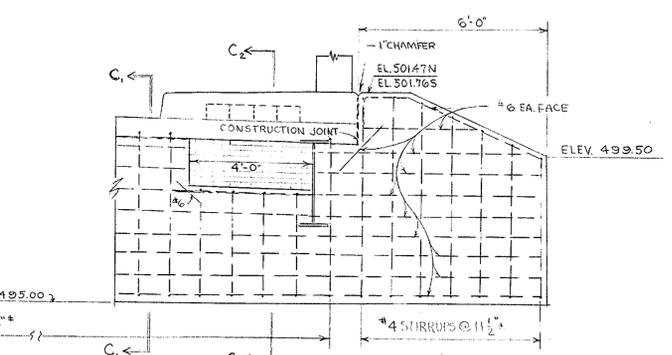
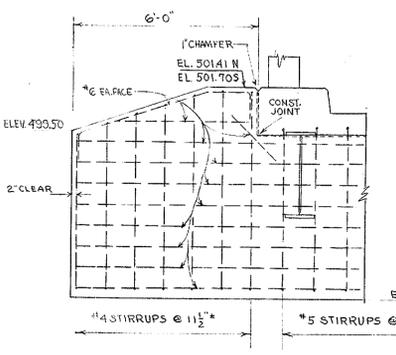
CROSS SECTION  
SCALE 1/2" = 1'-0"



STRINGER SPLICE DETAILS  
SCALE 1" = 1'-0"

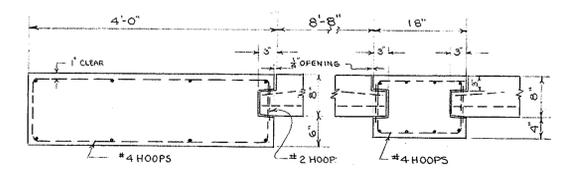


DETAILS OF FIXED BEARINGS  
SCALE 1" = 1'-0"

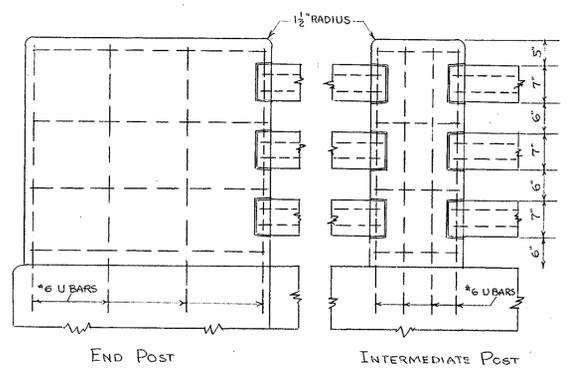


SECTION D1-D1  
(D2-D2 OPPOSITE HAND)  
SCALE 3/8" = 1'-0"

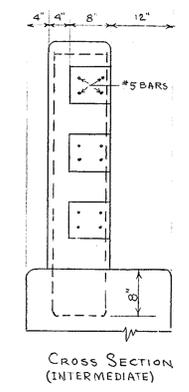
SECTION E1-E1  
(E2-E2 OPPOSITE HAND)  
SCALE 3/8" = 1'-0"



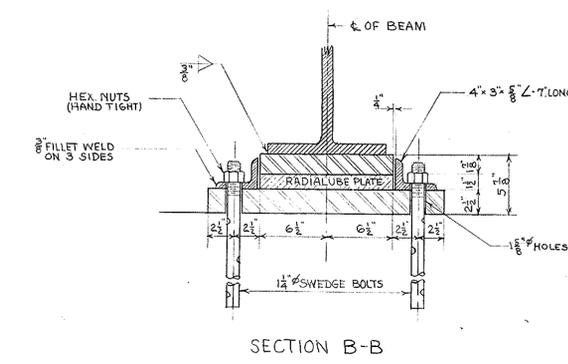
CONCRETE  
RAILS - CLASS E  
POSTS - CLASS D



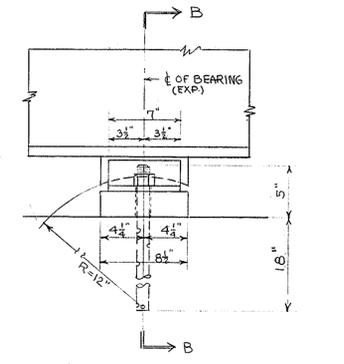
BRIDGE FENCE DETAILS  
SCALE 3/4" = 1'-0"



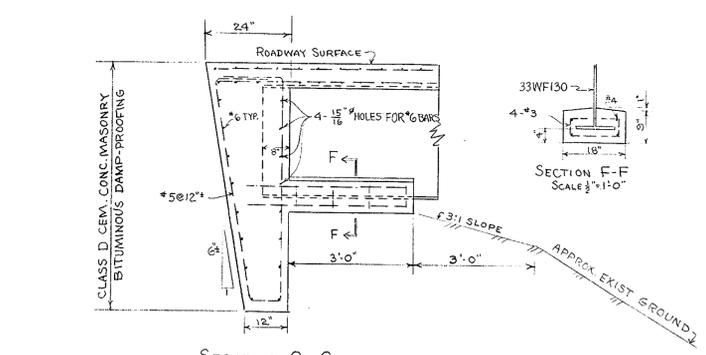
CROSS SECTION  
(INTERMEDIATE)  
SCALE 1" = 1'-0"



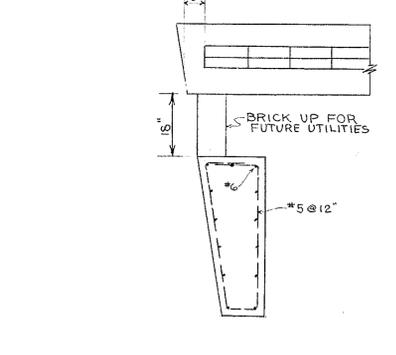
SECTION B-B



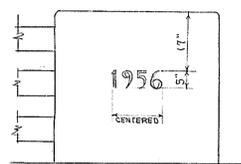
DETAILS OF EXPANSION BEARINGS  
SCALE 1 1/2" = 1'-0"



SECTION C1-C1  
SCALE 1/2" = 1'-0"



SECTION C2-C2  
OTHER DETAILS SAME AS SECTION C1-C1  
SCALE 1/2" = 1'-0"



LOCATION OF DATE  
SCALE 1/2" = 1'-0"

SEPT. 8, 1956 ISSUED FOR CONSTRUCTION  
DATE DESCRIPTION  
USE ONLY PRINTS OF LATEST DATE